

WARD: Southville CONTACT OFFICER: Jonathan Coombs

SITE ADDRESS: New Cut New Bridge Coronation Road/Cumberland Road Bristol

APPLICATION NO: 14/04262/FB Full Planning (Regulation 3)

EXPIRY DATE: 22 October 2014

New bridge to be constructed over River Avon New Cut, immediately opposite Camden Road, Southville.

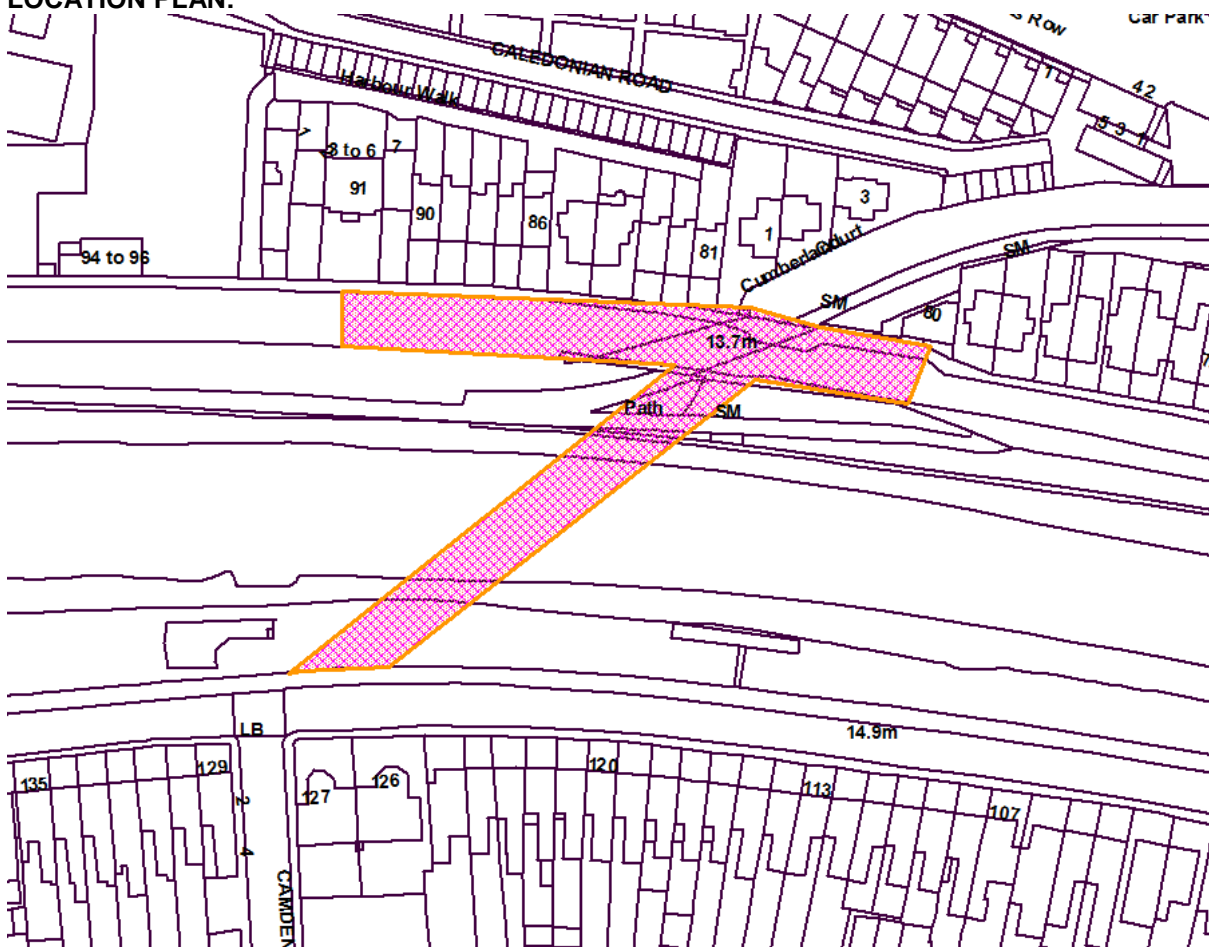
RECOMMENDATION: Grant subject to Condition(s)

AGENT: Bristol City Council
Brunel House
St Georges Road
Bristol
BS1 5UY

APPLICANT: Bristol City Council
Brunel House
St Georges Road
Bristol
BS1 5UY

The following plan is for illustrative purposes only, and cannot be guaranteed to be up to date.

LOCATION PLAN:



comtop
v4.0408

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REASON FOR REFERRAL

The application comes before members with a recommendation for approval, due to the balancing of heritage interests and transport benefits as well as an objection from Cllr Wright and the Civic Society as well as 8 members of the public.

SITE AND APPLICATION BACKGROUND

The application proposes a new cycle and pedestrian bridge across the new cut of the River Avon. This would stretch between Coronation Road, opposite Camden Road where a dropped kerb and tactile paving crossing point currently exists, and the Chocolate Path/Festival Way paths along the north side adjacent to Cumberland Road. The Festival Way goes underneath a road bridge for Cumberland Road to the harbourside, linking this with the Chocolate Path. The Harbourside Railway tourist attraction, operated by the City Council, follows the alignment of these two paths running west in between the Chocolate Path and Cumberland Road to the CREATE centre.

The Chocolate Path is a designated Public Right of Way under reference BCC/392/10 which ramps up to meet Cumberland Road to the east of the application site. The application site is within the City Docks Conservation Area, with the area south of Coronation Road falling within the Bedminster Conservation Area. The River Avon is designated a Site of Nature Conservation Interest, with the southern bank forming part of an area of important open space under Policy DM17. A series of nearby buildings are grade II listed, including Vauxhall Bridge, Nos. 81-90 and 91 Cumberland Road to the north of the application site, as well as No. 129 Cumberland Road, and Nos. 156-170 Coronation Road to the south-west of the application site. The Gaol Ferry Bridge is also identified as a landmark building within the City Docks Conservation Area Character Appraisal.

The bridge has come about as part of works associated with the awarding of £6.3m by central government in 2013 to the Cycle City Ambition Fund for a series of improved river and harbour crossings to increase cycling permeability to the city centre for residents south of the Avon New Cut. The application seeks to construct the bridge with extensive associated engineering works to facilitate the levels required for the landing to the northern side.

The works to the northern side consist of ramps down to meet the Chocolate Path to the west and east and also the Festival Way to the north just past the Cumberland Road bridge. The ramps would be formed by a retaining structure set back from the current river bank retaining walls of pennant stone by approximately 300mm with the existing railing removed due to difficulty of maintenance and the danger of trapping material in flood events. The ramp retaining walls as shown would be finished in a Staffordshire blue brick to match the existing Chocolate Path surfacing material colour and the capping bullnose brick to the river bank retaining walls. An indicative image has however been provided of pennant stone finish with vertical blue brick banding to the southern side of these structures for consideration. The concrete deck to the surface would be visible to the southern side. The existing surfacing material would be lifted to replace existing tarmac infill areas along the chocolate path and an epoxy mineral surface laid down over the proposed new ramped areas.

New railings would be introduced along this comprising of painted mild steel balusters and handrails with steel wire to 1.2m high to provide a safer means of enclosure over the current 1m high railings. To the northern side of the ramps the retaining structure continues in blue brick to form a boundary wall to secure the path in order to prevent steam from the Harbourside Railway burning users of the paths. The proposal seeks to use an epoxy bonded mineral surfacing material to provide a safer finish for use by cyclists and proposes to reutilise the Chocolate Path material removed elsewhere along the Chocolate Path where patch infilling/maintenance with tarmac has occurred. A small raised garden area is also proposed to the junction of the two paths that would integrate the existing commemorative bench by the founder of the Friends of the Avon New Cut.

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Works to the southern side are limited to installing a new crossing under the Council's permitted development rights and the details of this are to be finalised. An indicative plan has been provided of the possible resolution at the request of your officers.

The bridge itself is designed as a simple low-profile form to maintain views through to Cabot Tower from Camden Road and across the Conservation Area, as well as the primacy of the more historic bridges along the New Cut. This is formed by an a-symmetric box girder design (in section) that would provide a solid form to the eastern elevation and an open western elevation formed by railings to reflect the new proposed railings to the north retaining structures. It is proposed to finish this in umbra grey with the supports in oxide red to contrast with the vegetation and reflect the industrial/engineering heritage of the Conservation Area. This is supported by two concrete cast pier bases and a v-shaped support to the southern bank and a single column to the northern side within the river. The bridge would have a deck width of 4.7m at be ramped at a gradient of 1:28 down to the northern side. A navigable clearance designed to be no less than the River Avon road bridge clearance would be maintained for a 20m width at a height of 11.85m AOD.

During the lifetime of the application various amendments in addition to those listed above have been carried out to meet principally engineering requirements. These include strengthening of the pier, details of piles and the removal of the Chocolate Path railing between the two retaining structures to reduce maintenance challenges and potential for trapping material during flood events.

RESPONSE TO CONSULTATION AND PUBLICITY

Neighbour consultations were erected alongside a series of site notices and a press notice. 36 representations were received with 25 in support, 8 in objection and 3 neutral comments. These include comments from Cllr Wright, Cllr Bolton, the Bristol Civic Society, BS3 Planning Group, Southville Community Development Association (SCDA) Environment Group, the Greater Bedminster Community Partnership and Bristol Tree Forum that are set out in greater detail below.

The consultation response can generally be summarised as supportive of the principle of the bridge and the transport benefits this offers but with objectors/neutral comments raising concerns over the design option selected or detailed design matters and the relationship with the Conservation Area setting. For the sake of clarity it is noted that only the option before members can be considered under this application and a refusal would have to be justified on the scheme being unacceptable and would not be warranted on the basis of 'preferable alternatives'. The issues raised are summarised in more detail below:

- Support new pedestrian/cyclists footbridge to improve connectivity, reduce pedestrian/cyclist conflict as at Gaol Ferry Bridge, encourage use of Coronation Road cycle path and improve crossing access along this.
- The proposed design looks utilitarian and dull. A visually stunning and iconic bridge should be sought.
- Support the design as the new cut should take primacy/a suspension bridge would be too intrusive and out of keeping with the two adjacent bridges.
- Significant concerns over the extent of the building up of the river bank to make the bridge gradients work. The ramps will obstruct through views to the harbour from the Chocolate Path and enclose this space making it feel intimidating.
- The wall to the railway is uncharacteristic and should be replaced with railings. This wall would also encourage users to walk along presenting a safety risk.
- The blue brick should be replaced with pennant stone to match other retaining structures in the area, which would also deter graffiti.
- The Chocolate Path paving should be retained and the paving of the ramps matched as a feature of the Conservation Area.

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- Two comment in support raised concerns about the safety of the Chocolate Path. One noted that the paving material to the Chocolate Path catches water that freezes in winter causing a safety hazard and requested that the scheme connect in with Cumberland Road to enable users to avoid the Chocolate Path in such conditions. Another commenter considered that the
- Chocolate Path was unfit for purpose due to being uneven, infilling with concrete and tarmac, slippery and dangerous.
- Any railings lost should be replaced on a like for like basis.
- Speed limit on Coronation Road should be reduced to 20mph to safeguard users.
- Concern over risks of subsidence.
- View from commemorative bench relocation will not reflect original siting choice and could attract inconsiderate users. A bin should be provided to discourage littering. The location of the bench should be agreed after completion with the Friends of the Avon New Cut.
- The retaining walls should be designed to be graffiti proof.
- No pro-active consultation about the ramps.
- Site clearance is excessive and ignores the wildlife corridor.

The BRISTOL TREE FORUM have requested that tree group G5 be retained as far as possible.

The BRISTOL CIVIC SOCIETY supports the proposal to build a new bridge at this location but oppose the current application for the following reasons:

- A more interesting bridge design should be sought and would prefer option 3 of those presented within the design and access statement
- The ramps to the northern bank require further consideration as these will have a significant visual impact. The ramps should abut the path and not interrupt it.
- The existing surface material to the Chocolate Path should be integrated into the scheme and this paving remains available for use, although the re-instatement of the Chocolate Path paving to the east is welcomed.
- The impact of the works is difficult to comprehend without computer generated images or other illustrations.
- The area under/from Cumberland Road bridge would be enclosed by large retaining structures on three side and be dark and gloomy. Railings should replace the wall to the railway side of the ramps. Pennant stone is sought in place of the blue brick.
- The G5 tree group should be retained and replacement planting required to mitigate its impact.
- The ramp will encourage acceleration by cyclists and cause a potential hazard for pedestrians.
- The loss of approximately 100m of the paving material and traditional fence alongside the railway. These are key features of the Conservation Area and should not be lost.

The GREATER BEDMINSTER COMMUNITY PARTNERSHIP made the following comments in support of the application:

“Greater Bedminster Community Partnership support the proposed new foot and cycle bridge across the Avon New Cut from Coronation Road to the Chocolate Path. We believe that the new bridge will encourage walking and cycling and support the partnerships aim to encourage sustainable travel.

The partnership recommends and requests that;

1. A protected crossing of Coronation Road in the area of Camden Road be provided due to the severe difficulties faced by pedestrians and cyclists.
2. That the new bridge is linked into existing and planned cycling/walking routes in Greater Bedminster, notably the 'green-walkways' and through legible city signage. The partnership requests that council travel planners work with the GBCP to ensure these element dove-tail together.
3. Existing natural history including trees should be protected and suitable mitigation be provided where there is a loss of trees or other vegetation.”

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The SOUTHVILLE COMMUNITY DEVELOPMENT ASSOCIATION (SCDA) ENVIRONMENT GROUP supported the principle of the bridge but also raised the following comments about wider integration of any development:

- A new protected crossing of Coronation Road should be provided even if the bridge were not to go ahead due to the difficulties of crossing by pedestrians and cyclists.
- Improvements to mapping, signing and walkway network are being promoted by the Greater Bedminster Community Partnership. Any new bridge should be integrated into this. [These details have been passed onto the agent]
- Existing natural history such as trees should be protected and suitable mitigation provided where there is a loss.

The BS3 PLANNING GROUP advised that they were not convinced that the design of the structure as proposed is appropriate and that there had been minimal/no consultation about what will be a new strategic structure over the Cut.

CLLR MARK WRIGHT made the following comments in objection:

"I support the principle of a new cycling (or foot) bridge at this location to relieve congestion at Gaol Ferry bridge and feed the Chocolate Path.

However, the proposed design is not good enough. The views down the New Cut, particularly from Gaol Ferry Bridge, are iconic and should be respected. The thick and heavy grey proposed design is poor and would harm those iconic views.

The architects should note the visual success of Gaol Ferry Bridge itself in particular the way it is structurally transparent and so appears lighter and more elegant to the viewer. This is due to the structure being just the supporting skeleton framework with no panelling. Valentine's Bridge next to Temple Meads is also a good example.

I support also the comments by the Civic Society regarding the join between the bridge and the existing path."

CLLR CHARLIE BOLTON made the following comments in support:

"I strongly support the building of this bridge.

I think it will be extremely popular with Southville and Ashton residents and will become a significant walking and cycling route into town. It therefore has obvious benefits in terms of helping sustainable transport in the city.

- I support the resolution passed by the Greater Bedminster Community partnership which states
- '1. A protected crossing of Coronation Road in the area of Camden Road be provided due to the severe difficulties faced by pedestrians and cyclists.
 2. That the new bridge is linked into existing and planned cycling/walking routes in Greater Bedminster, notably the 'green-walkways' and through legible city signage. The partnership requests that council travel planners work with the GBCP to ensure these element dove-tail together.
 3. Existing natural history including trees should be protected and suitable mitigation be provided where there is a loss of trees or other vegetation.'

I have read other comments and do not particularly agree that the design of this bridge is any worse than the other options considered.

I note a comment about the impact on the Chocolate Path, but can't tell from the design whether this is so. Even so, I regard the building of the bridge as more important.

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Finally, I would also support at least a partial closure of that end of Camden Road to traffic, and we take the opportunity to create a people-friendly entry point to Southville.”

OTHER COMMENTS

BCC Transport (Development Management) Team have made the following comments:-

Principle

The new bridge is acceptable in principle providing a new pedestrian and cycle river crossing between Coronation Road, the Chocolate Path and Cumberland Road. Therefore removing a current and substantial barrier to cycle / pedestrian movements in the area, and meeting Local Plan policies BCS1, BCS2, BCS10, BCS13, DM22 and DM23.

Structure / Design

Detailed plans of the bridge and its supports must be submitted and approved by the Local Authority based on suitable surveys.

The bridge must make provision for access for maintenance works and inspection with these details sought by condition. This would be anticipated to include provision for access to the underside for works and inspection including for scaffold fixing points, and to allow for underslung scaffold and works loads.

Coronation Road and Chocolate Path Connections

Detailed construction plans regarding the bridge connections to the footways on Coronation Road and the Chocolate Path will need to be agreed with the highways and structures teams as part of the detailed design process. These should include necessary footway and river wall repair and upgrade works where necessary based on suitable surveys. A condition will be required to ensure that this dialogue takes place prior to construction.

Lighting

A detailed lighting design for the bridge with suitable lighting must be approved by the Lighting Team prior to development as the LED lighting may be unsuitable for adoption.

Again this will be required to be agreed subject to a condition.

Adoption / Maintenance

It is anticipated the bridge will become adopted highway infrastructure therefore details of construction to achieve an adoptable standard need to be submitted and approved in writing by the Local Planning Authority prior to development. On confirmation that the bridge will be adopted as a highway asset it will be necessary to agree a maintenance programme with the Highway Authority.

Construction Management

A construction management plan will be sought by condition prior to works commencement to ensure highway safety.

BCC Archaeology have made the following comments:-

From a historic environment point of view the principle of a new bridge in this location was agreed prior to the submission of this application. At this stage it was suggested that detailing of the new structure should have high regard for the character of the two conservation areas and New Cut as a whole. It was also suggested at an initial stage that the bridge should be 'light-weight' in appearance.

The only significant heritage issue is the impact that the proposed new bridge and associated structures will have on the character of the area.

It is accepted that for engineering reasons one side of the bridge will have to be a solid feature. However, efforts should be made to break-up the visual mass of the bridge as viewed from

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Cumberland Road. This can be achieved through the use of vertical details on the east/outward facing elevation in conjunction with a recessive paint scheme.

The use of engineering bricks for the retaining structure of the ramped approach on the Cumberland Road side was previously discussed. However, the details shown in the application suggest that this will be a visually intrusive feature because of its scale when viewed from the south. It is suggested that the use of varying materials in keeping with the character of the local area could help to break up this visual massing.

The only significant impact on historic fabric will be the removal of a section of the historic chocolate path. It is noted that it is intended to lift and reuse some of these blocks, but the reuse of this material should extend further to the east than currently shown so that it meets Cumberland Road.

These issues of details can be resolved through appropriate conditions and colleagues from urban and landscape design can comment on these.

I note the written scheme of investigation provided by Bristol and Region Archaeological Services with the presumption that a watching brief will be required. This archaeological monitoring can be secured through attaching the conditions B28 and C18 to any consent.

BCC Landscape Design have made the following comments:-

The comments below are intended to cover both issues arising from both landscape and urban design concerns. The principal matter is that of fitting the new construction - bridge and ramps – within the historic harbour context and its impact on Conservation Area character through changes in layout and fabric.

- New ramp wall elevations
 - o Engineering brick detailing should be relieved with pennant stone panels to match as far as practicable the harbour wall - the brick pilaster detail could be used to define the stone panels.
 - o There is no need to retain the existing railings along the length of the new ramps.
 - o The rear of the ramp wall facing the railway could be detailed as a render finish between the pilasters to offset costs arising from inclusion of stone above and provide a lighter, more reflective surface.
 - o I think that the south elevation detail option provided is a great improvement in terms of appearance and impact on the character of the conservation area.
- Surface finishes
 - o The resurfaced chocolate path detail should be extended to the top of the ramp on the Cumberland Road
 - o The red granite cycle path will be visually intrusive and unnecessary in this location and should be omitted as detailing will include a feature denoting segregation of pedestrians and cyclists.
 - o As a principle the chocolate pavers should be used wherever sub-base conditions permit; current drawings show lengths of bauxite surfacing where the original surfacing might be used.
 - o Chocolate path edge detail should be used in areas of bauxite paving to retain the link with the original historic fabric of the route
- Placemaking details
 - o A more resolved layout drawing is required here indicating how detailed levels and paving surfaces come together.
 - o What is the material treatment of the raised paved level?
 - o Street furniture; bollards on the Coronation Road abutment to be of a design suitable to historic harbourside setting.

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- Landscape within the D & A statement. Soft landscape proposals within the D&A are currently descriptive – detailed design of new planting including replacement tree replacements should be provided at this stage to ensure the retention of amenity and habitat.

BCC Nature Conservation have made the following comments:-

This proposal directly affects the River Avon Site of Nature Conservation Interest (SNCI), River Avon. Policy DM19 in the Local Plan applies. The red line planning area includes an area of coastal saltmarsh which is a priority habitat listed on Section 41 of the Natural Environment and Rural Communities Act (2006). This means that the presence of this habitat is a material consideration in planning decisions. A Construction Environment Management Plan is required to ensure that impacts upon the SNCI and salt marsh are minimised/mitigated.

The ecological survey report dated March 2014, highlights the potential presence of slow-worms on this site (page 12). Slow-worms are legally protected against being killed or injured. A condition requiring a method statement for clearance is required. A condition requiring compliance with the recommendations of the ecological survey should also be attached to any permission.

A landscaping scheme should be conditioned and this should take account of the recommendations in the submitted ecological survey report dated March 2014, with respect to saltmarsh (a priority habitat), landscaping and habitat creation.

Due to records of the presence of otters within the River Avon a condition is recommended preventing works outside of daylight hours. Details of the lighting should also be controlled by condition to minimise any harm to bats/nocturnal species

The design of the bridge minimises the potential for collisions by mute swans which have been recorded using the River Avon New Cut. This involves avoiding cables in the design of the bridge and the use of a contrasting colour scheme to the sky should be considered on the bridge.

Trees are likely to be removed as part of this proposal. All species of wild birds, their eggs, nests and chicks are legally protected until the young have fledged. A planning condition is recommended requiring evidence from an ecologist for works during bird nesting season. The extent of vegetation lost to the southern bank may impact upon foraging and commuting bats. This should be minimised by retaining as much of the existing vegetation as possible in accordance with the recommendations of the ecology report.

Natural England has commented as follows:-

The lack of specific comment from Natural England should not be interpreted as a statement that there are no impacts on the natural environment, but only that the application is not likely to result in significant impacts on statutory designated sites, landscapes. It is for the local authority to determine whether or not this application is consistent with national or local policies on biodiversity and landscape and other bodies and individuals may be able to help the Local Planning Authority (LPA) to fully take account of the environmental value of this site in the decision making process, LPAs should seek the views of their own ecologists when determining the environmental impacts of this development.

We would, in any event, expect the LPA to assess and consider the possible impacts resulting from this proposal on the following issues when determining this application:

- Protected species
- Local sites
- Impact Risk Zones for Sites of Special Scientific Interest

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BCC Arboricultural Team has commented as follows:-

The proposals include a basic tree plan and a Preliminary Tree Schedule. In principal the removal of some of the bank planting is acceptable to accommodate a bridge structure across the new cut. However we will need more detailed information on the effect of the tree population in this area and mitigation measures to overcome the negative impact of tree removals.

We require details of the trees that will be affected (BS5837 category, species, stem size) clearly showing trees to be removed.

Any tree protection measures (To British Standard 5837) for retained trees to be clearly shown on a plan.

Proposed compensation planting numbers as calculated under the Bristol Tree Replacement Standard.

A landscape plan clearly showing tree replacement proposals including tree location, species, size, protection (if required) establishment /maintenance schedule and tree pit specification (if trees are planted into hard surfacing) is required.

I am not sure that there is sufficient space around the site to fully comply with forecasted replanting requirements therefore a Unilateral Undertaking may be necessary to provide funding for tree planting elsewhere.

Environment Agency (Sustainable Places) has commented as follows:-

We have considered the Flood Risk Assessment (FRA) provided and an additional FRA addendum provided by the applicants (email from Tomas Meyrick, 10/11/2015).

Based on this information we have no objection to the development proposed. However, further detail of the bridge design, particularly the north bridge pier, will be required through the Flood Defence Consent process. We have therefore requested the following informative for any permission given. The detailed design will also have to consider the geomorphological implications of the scheme, particularly in relation to the potential for turbulence and scour for the two pier structure.

Informative(s):

Under the terms of the Water Resources Act 1991 and the Land Drainage Byelaws, the prior written consent of the Agency is required for any proposed works or structures in, under, over or within 8 metres of the top of the bank of the River Avon, designated a 'main river'. The need for Flood Defence Consent is over and above the need for planning permission. To discuss the scope of our controls and to obtain an application form please contact Colin Taylor on 01278 484610.

Avon and Somerset Constabulary Crime Reduction Unit have no objection to the proposal in verbal comments during their weekly surgery.

RELEVANT POLICIES

National Planning Policy Framework – March 2012

Planning (Listed Buildings & Conservation Areas) Act 1990

Bristol Local Plan, Adopted December 1997

CC7 Pedestrian Links

Bristol Core Strategy (Adopted June 2011)

BCS1 South Bristol

BCS2 Bristol City Centre

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BCS9	Green Infrastructure
BCS10	Transport and Access Improvements
BCS15	Sustainable Design and Construction
BCS16	Flood Risk and Water Management
BCS21	Quality Urban Design
BCS22	Conservation and the Historic Environment
BCS23	Pollution

Bristol Site Allocations and Development Management Policies (Adopted July 2014)

DM1	Presumption in favour of sustainable development
DM14	The health impacts of development
DM15	Green infrastructure provision
DM17	Development involving existing green infrastructure
DM19	Development and nature conservation
FDM21	Development of private gardens
DM22	Development adjacent to waterways
DM23	Transport development management
DM26	Local character and distinctiveness
DM27	Layout and form
DM28	Public realm
DM30	Alterations to existing buildings
DM31	Heritage assets
DM34	Contaminated land
DM37	Unstable land

Bristol Central Area Plan (emerging)

BCAP30	Pedestrian routes
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Supplementary Planning Documents

Planning Obligations - Supplementary Planning Document - Adopted 27 Sept 2012

Supplementary Planning Guidance

City Docks Character Appraisal

KEY ISSUES

(A) IS THE PROPOSAL ACCEPTABLE IN LAND USE TERMS/DOES THE PROPOSAL ADEQUATELY ADDRESS TRANSPORT AND MOVEMENT?

The southern bank of the river forms part of an area of important open space under Policy DM17 of the SADM. This sets out that development upon such spaces is only acceptable where this is ancillary to the space. The proposal seeks a new bridge to cross the River Avon and this is considered ancillary when assessed against the context the river bank with associated transport infrastructure/functions alongside it and the limited area of the bank required for the development.

The National Planning Policy Framework (NPPF - March 2012) states that encouragement should be given to solutions which support reduction in greenhouse gas emissions and should support the use of sustainable modes of transport. It advises that transport policies have an important role to play in facilitating sustainable development and contributing to wider sustainability and health objectives.

Policy BCS10 of the adopted Bristol Core Strategy (BCS June 2011) identifies that the council will support the delivery of transport infrastructure and that this includes a network of routes to encourage walking and cycling. The application site has no site specific allocations for transport infrastructure within either the adopted Site Allocations or Development Management Policies (SADM - July 2014), the saved policies of the Bristol Local Plan (BLP - 1997) or the publication version of the Bristol

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Central Area Plan (BCAP - February 2014) other than for the recently granted MetroBus scheme along the Cumberland Road, which has no impact upon the proposed development.

The crossing route is also not identified within the 'City Centre Public Realm and Movement Framework' which has informed the Bristol Central Area Plan's development, but this document does refer to improved pedestrian and cycle connections into the city centre from the 'inner access loop road' (that includes Coronation Road).

SADM Policy DM22 sets out that development adjacent to walkways will be expected to maintain, enhance or create public connections for walking/cycling and maintenance, including links to the public realm network. SADM Policy DM23 sets out that improvements to walking and cycling will be expected of development. Saved BLP Policy CC7 states that development along routes providing access to transport interchanges and radial routes providing connections with surrounding areas will be permitted.

Gaol Ferry Bridge is the busiest pedestrian/cycle bridge in the city with over 7,000 movements a day, of which over 3,000 are cycle journeys. This route is utilised by citizens living beyond the immediate surrounds for commuting journeys as well as recreationally. The bridge was never designed for cycle and foot traffic and its limited width does result in conflict between users as reflected in the consultation response. The scheme comes alongside various improvements to cycle routes at Wapping Wharf and north and south side of Princes Bridge. The proposed bridge would therefore provide for improved links across the Avon New Cut, linking in with the Festival Way and Chocolate Path. This would also provide for a dedicated cycle crossing where currently users must travel to either Gaol Ferry Bridge to the east (see above) or to Ashton Avenue Bridge to the west to avoid the steps of the Vauxhall Bridge. The bridge would also provide a further crossing point to Southville increasing the permeability of the area and providing the opportunity for further cycle route integration.

It is accepted that the bridge is wholly supported in principle by the consultation response and expert advice from BCC Transport and development management officers on the basis of the benefits above. It is also clear that the ramped sections are an engineering requirement to deliver a bridge in this location due to the change in levels between the banks and need for navigational clearance across the river to be maintained.

In respect of the detailed design, the width of the bridge is 4.7m with 2.7m designated for cyclists and pedestrian to the open side of the bridge to cater for those wishing to linger and enjoy the views. The railings/side of the bridge would be slightly angled/projecting to provide further width above the surface level dimension. The ramped sections would be between 3-4m in width to reflect the existing pathways connected to and the landing points made wider to minimise the potential for conflicts. The proposed ramps meet standard guidance of level changes of 1:20. The epoxy bonded mineral surface would provide a far preferable surfacing material in respect of comfort for cycling and reduced chances of slipping/icing and it is noted that some of the consultation response raised concerns about the suitability of the current Chocolate Path material. The proposed railings are above the 1m existing railings along the Chocolate Path in accordance with best practice on safety standards at 1.2m to the river banks and 1.4m in the form of a blue brick wall to the railway to prevent the transfer of any steam to users of the path during the operation of the Harbour Railway.

An indicative crossing is included within the submission to Coronation road and final details of this can be secured by condition to ensure that the development provides for a safe means of access/connectivity to/from the proposed bridge.

Final details of lighting are to be secured by condition and details of final detailed engineering arrangements and maintenance connections are to be agreed by condition to ensure long-term maintenance can be secured with the Council as operator.

The impact on the Public Right of Way is also a material planning consideration with Policy DM23

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stating that development will be expected to protect and enhance the function and amenity of public rights of way. The scheme would result in a temporary closure of part of the Chocolate Path to carry out works, but would ultimately have minimal impact beyond the self-evident improved connectivity of this route and the introduction of additional gradients for the ramped areas. This is considered acceptable.

Overall, the proposed development would deliver improved transport infrastructure by improving permeability across the River Avon New Cut for both pedestrian and in particular cyclists. This provides valuable benefits that should be given significant weight in determining the application.

(B) DOES THE PROPOSAL SAFEGUARD HERITAGE ASSETS AND ADEQUATELY ADDRESS DESIGN ISSUES?

This key issue is the principle element of debate raised from the consultation response. While objectors recognised the value of the proposed bridge in principle, they raised concern over the detailed design approach. The application site sits within the City Docks Conservation Area and is also located adjacent to the Southville Conservation Area and nearby a series of grade II listed buildings/structures.

Section 66 of the Planning (Listed Buildings and Conservation Areas) Act 1990 states that in considering whether to grant planning permission for development which affects a listed building or its setting, the local planning authority shall have special regard to the desirability of preserving the building or its setting or any features of special architectural or historic interest which it possesses.

The Authority is also required (under Section 72 of the Planning (Listed Buildings and Conservation Areas) Act 1990) to pay special attention to the desirability of preserving or enhancing the character or appearance of the conservation area. The case of *R (Forge Field Society) v Sevenoaks DC* [2014] EWHC 1895 (Admin) (“Forge Field”) has made it clear where there is harm to a listed building or a conservation area the decision maker “must give that harm considerable importance and weight.” [48].

Section 12 of the NPPF states that when considering the impact of a proposed development on the significance of a designated heritage asset, great weight should be given to the asset's conservation, with any harm or loss requiring clear and convincing justification. Paragraph 132 of the NPPF states that significance can be harmed or lost through alteration or destruction of the heritage asset or development within its setting. Further, Para.134 states that where a proposed development will lead to substantial harm to or total loss of significance of a designated heritage asset, local planning authorities should refuse consent, unless it can be demonstrated that the substantial harm or loss is necessary to achieve substantial public benefits that outweigh that harm or loss.

In addition, the adopted BCS Policy BCS22 and SADM Policy DM31 seek to ensure that development proposals safeguard or enhance heritage assets in the city. Policies DM26-27 also cover wider design matters.

The City Docks Conservation Area Character Appraisal sets out a series of views. This can be summarised as a panoramic views from Coronation Road/Camden Road to Cabot Tower, The Point and the Wills Memorial Tower; glimpsed views nearby along Coronation Road between the vegetation to Cabot Tower and the Wills Memorial Building, and; a long view to St Mary Redcliffe from Cumberland Road. The Character Area within this document notes the views up and down the new Cut as a strength of the area, as well as the Victorian Railings along the railway line and the Chocolate Path as key features of the Conservation Area. Loss of traditional features such as railings are identified as a threat, with restoration and reinstatement of railings along the railway identified as an opportunity.

The applicant has also submitted an assessment of local views of the proposed development to assist members. Due to vegetation, good views are limited to principally the west from Vauxhall Bridge, the

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Chocolate Path and sections of Cumberland Road. Some limited views are available from Gaol Ferry Bridge due to the bend in the River Avon New Cut.

The scheme would have some impact upon the setting of the nearby listed structure of Vauxhall Bridge and the listed buildings along Cumberland Road. The diversity of views to Vauxhall Bridge (and to a lesser extent Gaol Ferry Bridge) would be improved by the scheme, but it would interrupt views from the structures of the Avon New Cut. Of considerable less value is the impact to the setting of the listed buildings on Cumberland Road, which would be marginally effected from across Coronation Road by virtue of the new structures to the Chocolate Path beneath/nearby these listed buildings.

While the scheme is to be assessed in its totality it can be broken down into three different matters; the design of the bridge itself, the design of the ramps and surfacing material/railings for ease of presenting this report.

To summarise the legal and policy obligations before members of the committee, in assessing these matters, they must assess whether the proposal would result in harm to any heritage assets and give this considerable weight where affecting the Conservation Area or listed buildings. Members must be satisfied that if they consider any harm 'less than substantial' that there are justifications that outweigh this, or if they consider that any harm is 'substantial' that it is outweighed by the public benefits that are identified within key issue A.

(i) Bridge

In respect of the bridge members must be clear that they are assessing the acceptability of the scheme before them and not selecting the most preferable design option. In assessing the design it is important to note that there are a number of factors that have influenced the design decisions taken. This includes the ramping requirements for navigational clearance and suitable access for users of the bridge referenced above. In addition, engineering requirements and the historic subsidence of the New Cut retaining walls have limited the amount of loading to the north bank countenanced in the design development. The design process dismissed cable stayed options due to the potential for overshadowing of properties to the north, impact upon the panoramic view from Camden Road and the potential for wildlife conflicts, although officers acknowledge that such impacts would be limited.

Pre-application advice from the Bristol City Design Group was for a simple lightweight structure to minimise its impacts. The engineering requirements necessitate a solid structure to carry the load and this was accepted by BCC City Design Group officers. This has resulted in the less visible east side being formed by a solid section. The box girder form has the benefit of providing a more simple bridge form to retain the primacy of the nearby landmark bridges as well as the view from Camden Road. The materials and finishing colours would be emblematic of the industrial heritage of the Conservation Area. Officers therefore consider that the detailed design of the bridge, while simple, is appropriate to the context and the brief provided by the Council's design officers for the location. Officers have requested further visualisations to assist members at the committee and the agent has advised that these will be available for the committee.

The use of a bright red finishing colour to the surface treatment of the bridge to identify the cycle route is considered inappropriate in accordance with advice from the City Design Group and can be achieved by more simple means. A variation condition will ensure a more suitable finish is provided.

(ii) Ramps

The Bristol Civic Society initial comments raised queries over the ability of the existing Chocolate Path to be retained alongside any access ramps and this is not feasible. The chief concern over the ramps is due to their scale (over a 100m stretch up to 2.5m high) and the use of a blue brick finish. The existing surfacing material is blue and is also matched by the bullnose bricks to edge this onto the

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River Avon New Cut retaining walls. It is however noted that retaining structures within the area are otherwise formed by pennant stone and this forms part of the character of this part of the Conservation Area.

In discussing this matter with the agent, the architect for the scheme has advised that he considers a different material is warranted to reflect the phases of development rather than mimic the existing retaining walls. This is to an extent agreed, but the use of such an expanse of blue brick was of concern to officers and the agent was requested to consider an alternative. The agent has been unable to prepare a full suite of revised drawings ahead of committee, but has provided a sample elevation of the new retaining structure compiled by pennant stone with blue brick vertical bands to break up the massing of this element and differentiate it from the existing new cut retaining structure. This is considered a suitable approach. Final revised elevations can be sought by way of a variation condition.

The use of blue brick to the Harbour Railway is considered of less impact due to its smaller extent and that this effects only private views. This does however remain a tourist attraction and the dog-tooth pattern would add some visual interest to this retaining wall as well as deterring graffiti.

(iii) Surfacing materials/railings

The surfacing material to the Chocolate Path is not explicitly referenced as of value within the above referenced character appraisal. It can however be inferred, given that it is from this material that the path gains its name. The scheme would result in the removal of a 100m section of such material to be replaced with an epoxy bonded mineral surface. This is considered to result in less than substantial harm to the Conservation Area.

The proposed surface would be a preferable material for traversing by pedestrians/cyclists which is the function of the bridge. The removed surfacing material would also be used in infilling existing patchwork elsewhere along the path and this can be secured by condition, such that there would overall be no loss of this traditional surfacing material across the path and the impact upon the Conservation Area can be said to be neutral as a result from the removal of this material. The new retaining structures and railing would also form a coherent 'new' section to the path where a break in surfacing materials would be more appropriate.

The proposal would result in the loss of traditional railings to both the river bank and railway. Of these the railway railings are considered to be a feature of the Conservation Area. While the simple railings to the river bank are consistent along the Chocolate Path, these are later additions and also below best practice standards for safety. The loss of the railings between the proposed new retaining wall and existing retaining wall is considered acceptable, with the bullnose blue brick still referencing the existing alignment of the Chocolate Path. The City Design Group consider the new railing design acceptable and its continuance from the new bridge along the new retaining structures and surfacing would be coherent with distinguishing the new intervention from the original parts of the Chocolate Path.

The continuance of the blue brick retaining structure to form a parapet wall of 1.4m high would result in the loss of the historic railings and this does constitute less than substantial harm to the Conservation Area. This is however necessary due to the use of the Harbour Railway by steam engines and the elevation of the footpath resulting in a clear safety risk to users of the path. This is therefore considered to be suitable justification.

BCC City Design Group have also advised that further details are required in respect of integration of existing and new pathways, the treatment to the raised garden area and details of any street furniture. These matters can be addressed by condition. Details of landscaping have also been requested at application stage and the agent has advised that these will have been completed by the time of committee and this will be reported within the amendment sheet. The recommendation below currently contains a landscaping condition to cover both hard and soft landscaping details.

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The application also contains an archaeological desk top assessment. The site's location give rise to the potential for archaeological interest and BCC Archaeology have confirmed that a watching brief should be secured by condition.

Overall, it is considered that the scheme would result in less than substantial harm to the Conservation Area, but this harm is considered to be justified by the public benefit of the bridge and the detailed engineering/design constraints placed upon this by its environment. Officers therefore consider that the scheme adequately addresses this key issue subject to conditions.

(C) DOES THE PROPOSAL ADEQUATELY ADDRESS NATURE CONSERVATION ISSUES/TREES?

The site is within a Site of Nature Conservation Interest and protected species such as slow-worms, bats and otters are recorded within/nearby the application site. Policies BCS9, DM19 and DM22 refer, as well as under The Wildlife and Countryside Act 1981 (as amended) and The Conservation of Habitats and Species Regulations 2010 (as amended). The impacts from the development have been assessed in a detailed ecological report submitted with the application. These matters can be addressed through the use of appropriate conditions in accordance with BCC Nature Conservation comments above.

The scale of vegetation clearance is currently undefined as this would be resolved under the tendering process by final contractors. The submitted application therefore sets out a worst case scenario of 480m² to the southern bank and 400m² to the northern bank within drawing 203742.BV.00.46.002, final arrangements of this can be sought under the Construction Environment Management Plan secured by condition. The scheme results in the loss of trees T5, T6, T7, T9 and groups G4, G5 all classified as C grade and groups G2 and G3 classified as U grade (i.e. no mitigation required). The submitted design and access statement calculates the necessary tree replacement planting at 18 trees, with the existing commemorative tree replanted under the scheme rather than replaced. Officers, on review, have calculated the necessary replacements as 24 trees under the Bristol Tree Replacement Standard set by Policy DM17. A final landscape design is to be submitted prior to the committee date and details of this will be issued within the amendment sheet and officer presentation. The need for a recommendation subject to a memorandum of understanding may be required. It is clear however that compensatory planting is required to mitigate the loss of the vegetated banks irrespective of tree replacement numbers and this will be secured on any approval.

Conditions covering tree protection and replacement are included within the recommendation.

(D) DOES THE PROPOSAL MITIGATE ANY FLOOD RISK IMPACTS?

The scheme includes a Flood Risk Assessment with a further addendum provided at application stage to address Environment Agency queries about impacts upon flood water discharge impacts. The Environment Agency have no objection to the proposal and have advised that an informative be applied covering the need for consents from the Environment Agency due to the works affecting a waterway. A drainage condition will be secured in accordance with Policy BCS16 and Policy DM22 as well as BCC Transport comments.

(E) DOES THE PROPOSAL ADEQUATELY ADDRESS LAND CONTAMINATION AND STABILITY ISSUES?

Land contamination issues have not been raised as of concern by the Environment Agency and land stability matters are also covered by conditions to satisfy transport structures team due to future maintenance liabilities.

(F) DOES THE PROPOSAL GIVE RISE TO ANY AMENITY/SECURITY IMPACTS?

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The scheme has the potential to create additional graffiti surfaces, the use of a variation condition to secure undressed pennant stone would minimise this risk, alongside the use of a dog-tooth pattern brick inside the Harbour Railway wall.

CONCLUSION

The proposed development would generate clear transport infrastructure benefits for pedestrians and cyclists using the River Avon New Cut. On detailed review officers consider that the design of the bridge is appropriate, but some less than substantial harm to the Conservation Area would arise. This harm is justified by virtue of the clear public benefits and constraints of the site requiring the extensive ramping, walls to the railway line and new surfacing being desirable to fulfil the bridges role as a cycle route.

It is therefore recommended that the scheme be granted subject to conditions.

RECOMMENDED GRANT subject to condition(s)**Time limit for commencement of development**

1. Full Planning Permission

The development hereby permitted shall begin before the expiration of three years from the date of this permission.

Reason: As required by Section 91 of the Town and Country Planning Act 1990, as amended by Section 51 of the Planning and Compulsory Purchase Act 2004.

Pre commencement condition(s)

2. Construction Environment Management Plan

Prior to commencement of development a Construction Environmental Management Plan (CEMP), including a detailed method statement for works to minimise ecological impacts (including minimising the area of site clearance) on the SNCI and its constituent habitats including coastal saltmarsh, to be prepared by a suitably qualified ecologist, shall be submitted to, and approved in writing by the Local Planning Authority. The approved plan shall be implemented and adhered to thereafter at all times during construction.

This shall include best practice pollution control measures to ensure that adverse impacts (including dust and air pollution, effects on water quality, pollution from fuel use and storage and other potentially hazardous materials) do not occur on the SNCI as a result of construction works. The CEMP should include details of robust protective fencing incorporating warning signs to minimise impacts on the SNCI and its location and a plan showing the boundaries of the SNCI. Contractors and sub-contractors should be briefed on the importance of the ecological features which are to be retained on site and the ecological value of the SNCI prior to the commencement of works.

Reason: To conserve the Site of Nature Conservation Interest and coastal salt marsh.

3. Slow worms

Prior to clearance of the site, a detailed method statement for clearance works with respect to the potential presence of slow-worms, to be prepared by a suitably qualified ecologist shall be submitted to and agreed in writing by the local planning authority. Works shall then proceed

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in accordance with the agreed method statement. This shall include post-translocation monitoring of the receptor area or site if slow-worms are confirmed on site.

If slow-worms are found on site features to promote their conservation such as a hibernaculum and the retention or provision of suitable vegetation, should be incorporated within the method statement. Slow-worms may only be translocated (moved) when they are active, which is usually between April and September inclusive.

Reason: To ensure that if legally protected reptiles are present on the site that they are not harmed.

4. Compliance with ecology report

All site clearance and construction works shall be carried out in strict accordance with the recommendations (please see page 12) in the submitted ecological survey report dated March 2014, unless otherwise agreed in writing by the Local Planning Authority. This report includes recommendations with respect to saltmarsh (a priority habitat), landscaping and habitat creation.

Reason: In the interests of the protection and welfare of legally protected and priority species.

5. Clearance of vegetation

No clearance of vegetation or structures suitable for nesting birds, shall take place between 1st March and 30th September inclusive in any year without the prior written approval of the local planning authority. The authority will require evidence provided by a suitably qualified ecologist that no breeding birds would be adversely affected before giving any approval under this condition.

Reason: To ensure that wild birds, building or using their nests are protected.

6. Construction management plan

No development shall take place including any site clearance works until a construction management plan or construction method statement has been submitted to, and approved in writing by the Local Planning Authority. The approved plan/statement shall be adhered to throughout the construction period. The statement shall provide for:

- * Parking of vehicle of site operatives and visitors
- * routes for construction traffic
- * hours of operation
- * method of prevention of mud being carried onto highway
- * pedestrian and cyclist protection
- * proposed temporary traffic restrictions
- * arrangements for turning vehicles

Reason: In the interests of safe operation of the highway.

7. Further details before development starts

No development shall take place until detailed drawings of the following have been approved in writing by the Local Planning Authority. The detail thereby approved shall be carried out in accordance with that approval.

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- (i) Structural design of the bridge, bridge support, bridge connection and footway works, repositioning, upgrading and repairing walls, railings and other related structures based on suitable surveys.
- (ii) Lighting.
- (iii) Construction details and maintenance arrangements to the bridge and retaining structure to suit an adoptable standard.

Reason: In the interests of visual amenity within the Conservation Area and highway safety of the area.

8. Sustainable urban drainage system (SUDS)

No development shall take place until a detailed design of surface water drainage for the site using sustainable drainage methods has been submitted to and approved in writing by the Local Planning Authority. The approved development shall be implemented in accordance with the approved detailed design prior to the use of the building commencing.

Reason: To ensure that the principles of sustainable drainage are incorporated into this proposal.

9. To ensure implementation of a programme of archaeological works

No development shall take place until the applicant/developer has secured the implementation of a programme of archaeological work, in accordance with a Written Scheme of Investigation which has been submitted by the developer and approved in writing by the Local Planning Authority.

The scheme of investigation shall include an assessment of significance and research questions; and:

- * The programme and methodology of site investigation and recording
- * The programme for post investigation assessment
- * Provision to be made for analysis of the site investigation and recording
- * Provision to be made for publication and dissemination of the analysis and records of the site investigation
- * Provision to be made for archive deposition of the analysis and records of the site investigation
- * Nomination of a competent person or persons/organisation to undertake the works set out within the Written Scheme of Investigation.

Reason: To ensure that archaeological remains and features are recorded prior to their destruction.

10. Variations to the scheme

No development shall take place until detailed drawings showing the following modifications to the scheme shall be submitted to and approved by the Local Planning Authority:

- (i) Southern elevation of the proposed ramp showing the proposed penant stone treatment submitted under drawing 969.CRB.17 in place of the blue brick treatment
- (ii) Provision of an alternative surfacing treatment to the bridge to remove the red epoxy bonded mineral in place of a line and markings to denote cyclist and pedestrian areas.

This part of the development shall be completed only in accordance with the modification thus approved.

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Reason: The above details are considered an inappropriate treatment within the Conservation Area and this aspect of the scheme should be modified to ensure an acceptable form of development.

11. Protection of retained trees during the construction period

No work of any kind, including site clearance, shall take place on the site until details of tree protection have been submitted to and approved in writing by the Local Planning Authority. The details thereby approved shall be erected around the retained trees in the position and to the specification agreed and The Local Planning Authority shall be given not less than two weeks prior written notice by the developer of the commencement of works on the site in order that the council may verify in writing that the approved tree protection measures are in place when the work commences. The approved fence(s) shall be in place before any equipment, machinery or materials are brought on to the site for the purposes of the development and shall be maintained until all equipment, machinery and surplus materials have been removed from the site. Within the fenced area(s) there shall be no scaffolding, no stockpiling of any materials or soil, no machinery or other equipment parked or operated, no traffic over the root system, no changes to the soil level, no excavation of trenches, no site huts, no fires lit, no dumping of toxic chemicals and no retained trees shall be used for winching purposes. If any retained tree is removed, uprooted or destroyed or dies, another tree shall be planted at the same place and that tree shall be of such size and species, and shall be planted at such time, as may be specified in writing by the council.

Reason: To protect the retained trees from damage during construction and in recognition of the contribution which the retained tree(s) give(s) and will continue to give to the amenity of the area.

12. Submission and approval of landscaping scheme

No development shall take place until there has been submitted to and approved in writing by the Local Planning Authority a scheme of hard and soft landscaping, which shall include the following:

- (i) Details of all species to be planted, including their location and species as well as maintenance/establishment period (5 years) - to include at least 24 trees with details of stock sizes and tree pit specification where within hard standing. Species selection shall take account of the recommendations in the submitted ecological survey report dated March 2014, with respect to saltmarsh (a priority habitat), landscaping and habitat creation.
- (ii) Construction drawings showing in detail the proposed surface, edging and emerging with existing pathway details to the ramped pathways.
- (iii) Construction details of the proposed raised 'garden' area.
- (iv) Manufacturer's details of all hard surfacing treatments indicating material, colour and finish.
- (v) Manufacturer's details of all street furniture (including bollards and railings) indicating location, detailed appearance, material, colour and finish.

The approved scheme shall be implemented so that planting can be carried out no later than the first planting season following the occupation of the building(s) or the completion of the development whichever is the sooner. All planted materials shall be maintained for five years and any trees or plants removed, dying, being damaged or becoming diseased within that period shall be replaced in the next planting season with others of similar size and species to those originally required to be planted unless the council gives written consent to any variation.

Reason: To protect and enhance the character of the site and the Conservation Area, to ensure its appearance is satisfactory and to mitigate any nature conservation impacts.

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13. Sample panels before specified elements started

Sample panels of the pennant stone and blue brick pilasters agreed under the above variation condition and the blue brick dog tooth retaining structure demonstrating the colour, texture, face bond and pointing are to be erected on site and approved in writing by the Local Planning Authority before the relevant parts of the work are commenced. The development shall be completed in accordance with the approved details before the building is occupied.

Reason: In order that the external appearance of the building is satisfactory.

14. Approval of road works necessary

The bridge shall not be open to use until details of the following works to the highway have been submitted to and approved in writing by the Local Planning Authority:

- (i) Details of footway connection works
- (ii) Details of toucan crossing to Coronation Road

The bridge hereby permitted shall not be open to use until these works have been completed in accordance with the approved details.

Reason: To ensure that all road works associated with the proposed development are to a standard approved by the Local Planning Authority and are completed before occupation.

15. Completion of Pedestrians/Cyclists Access - Shown on approved plans

The bridge hereby permitted shall not be open for use until the means of access for pedestrians and/or cyclists have been constructed in accordance with the approved plans and shall thereafter be retained for access purposes only.

Reason: In the interests of highway safety.

16. Chocolate Path Paving

The removed chocolate path paving blocks shall be reutilised to make good areas of infilling/patchwork elsewhere along the Chocolate Path prior to the opening of the bridge for use. Details of the amount of surfacing material removed, its storage and areas identified for relaying shall be submitted to and approved in writing prior to the removal of this material.

Reason: In the interests of the visual amenity of the Conservation Area.

17. To secure the conduct of a watching brief during development groundwork's

The applicant/developer shall ensure that all groundwork's, including geotechnical works, are monitored and recorded by an archaeologist or an archaeological organisation to be approved by the council and in accordance with the Written Scheme of Investigation approved under condition 9.

Reason: To record remains of archaeological interest before destruction.

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18. Light plan

Prior to the installation of the bridge or associated ramps/railings, details for any proposed external lighting shall be submitted and agreed in writing by the Local Planning Authority. This shall include a lux level contour plan, and should seek to ensure no light spill outside of the site boundaries in accordance with paragraph 125 (page 29) of the National Planning Policy Framework (2012), 'By encouraging good design, planning policies and decisions should limit the impact of light pollution from artificial light on local amenity, intrinsically dark landscapes and nature conservation.'

Reason: To conserve legally protected bats and other nocturnal wildlife.

Post occupation management

19. Otters

To ensure that otters are not disturbed by construction activities, works associated with the development hereby approved, in, adjacent to, or impacting on the river shall operate during daylight hours only, ceasing operation one hour before sunset and not commencing until one hour after sunrise.

Reason: Otters are listed as European Protected Species which are protected by law against harm, damage or destruction of their breeding and resting places and from deliberate disturbance. They tend to be most active at night when they are most prone to being disturbed.

List of approved plans

20. List of approved plans and drawings

The development shall conform in all aspects with the plans and details shown in the application as listed below, unless variations are agreed by the Local Planning Authority in order to discharge other conditions attached to this decision.

001 P-01 Location plans, received 27 August 2014
 002 P-02 Site plan, received 21 November 2014
 003 P-02 Plan and longitudinal profile, received 21 November 2014
 004 P-01 V-shape pier and typical deck cross sections, received 27 August 2014
 005 P-01 North pier elevation and section, received 27 August 2014
 006 P-02 Sections AA and BB, received 21 November 2014
 007 P-02 Sections CC, DD and EE, received 21 November 2014
 008 P-02 Ramp and bridge levels, received 21 November 2014
 10A Reference plan and handrail details, received 21 November 2014
 11 Surface finishes plan, received 27 August 2014
 12A Ramp details, received 21 November 2014
 13A Placemaking details, received 21 November 2014
 TCP-1 Tree constraints plan, received 27 August 2014
 Design and Access Statement, received 27 August 2014
 Conservation Statement, received 27 August 2014
 17 Proposed South Wall Elevation - indicative pillasters and pennant stone option, received 21 November 2014
 Camden Road Bridge Assessment of Views, received 21 November 2014
 Flood Risk Addendum, received 11 November 2014
 Bat activity - addendum ecology survey, received 22 October 2014
 009P-01 Proposed Elevation GG, received 21 November 2014

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Reason: For the avoidance of doubt.

Advices

- 1 Retaining walls: Where retaining walls above or below the highway are to be constructed or are affected by development, details of the structural design should be approved by the Local Highway Authority. (Telephone 0117 9222100).
- 2 Works on the public highway: The development hereby approved includes the carrying out of work on the public highway. You are advised that before undertaking work on the highway you must enter into a formal agreement with the council which would specify the works and the terms and conditions under which they are to be carried out. You should contact City Development, Wilder House, Wilder Street, Bristol, BS2 8PH or telephone 0117 903 6846, allowing sufficient time for the preparation and signing of the agreement. You will be required to pay fees to cover the councils cost's in undertaking the following actions:
 - 1) Drafting the agreement
 - 2) A monitoring fee equivalent to 15% of the planning application fee
 - 3) Approving the highway details
 - 4) Inspecting the highway works.
- 3 Highway to be adopted: All or part of the highway to be constructed in accordance with planning approval hereby granted is to be constructed to an adoptable standard and subsequently maintained at public expense. It is necessary for the developer to comply with the Highway Engineer's specification and terms for the phasing of the development, in accordance with section 38 (Adoption of highway by agreement) or section 219 (the Advance Payments code) of the Highways Act 1980. You must also contact the Engineering Design and Main Drainage Design section of City Transport to discuss the requirements for adopted roads or sewers and in due course submit a separate application in respect of these works. You are reminded of the need for early discussions with statutory undertakers to co-ordinate the laying of services under highways to be adopted by the Highway Authority. Telephone 0117 9222100.
- 4 The development hereby approved is likely to impact on the highway network during its construction. The applicant is required to contact Highway Network Management to discuss any temporary traffic management measures required, such as footway, Public Right of Way or carriageway closures, or temporary parking restrictions. Please call 0117 9031212 or email traffic@bristol.gov.uk a minimum of eight weeks prior to any activity on site to enable Temporary Traffic Regulation Orders to be prepared and a programme of Temporary Traffic Management measures to be agreed.
- 5 Under the terms of the Water Resources Act 1991 and the Land Drainage Byelaws, the prior written consent of the Agency is required for any proposed works or structures in, under, over or within 8 metres of the top of the bank of the River Avon, designated a 'main river'. The need for Flood Defence Consent is over and above the need for planning permission. To discuss the scope of our controls and to obtain an application form please contact Colin Taylor on 01278 484610.
- 6 In respect of lighting you are advised to discuss the matter with BCC Lighting and BCC Nature Conservation to ensure a satisfactory solution that addresses transport and nature conservation interests.

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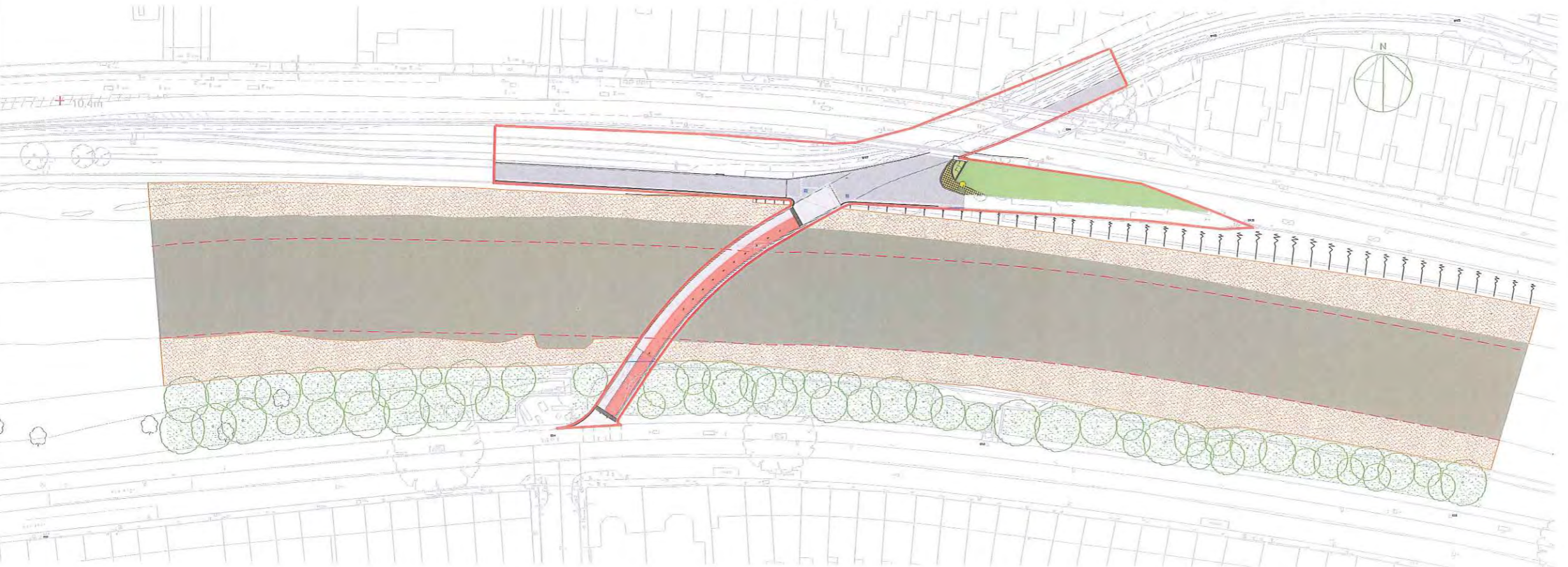
Application No. 14/04262/FB: New Cut New Bridge Coronation Road/Cumberland Road Bristol

BACKGROUND PAPERS

Natural England	22 October 2014
Transport Development Management	11 November 2014
Arboricultural Team	23 October 2014
Nature Conservation Officer	23 October 2014
Environment Agency (Sustainable Places)	11 November 2014
Archaeology Team	12 November 2014



LOCATION PLAN
Scale 1:1250

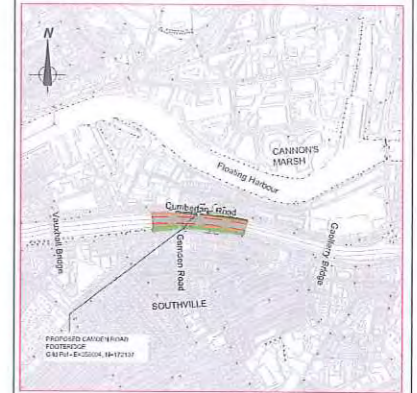


PLAN SHOWING
REDLINE BOUNDARY
Scale 1:500

FOR PLANNING



Location Plan:



NOTES

- ALL DIMENSIONS ARE IN MILLIMETRES UNLESS STATED OTHERWISE.
- ALL LEVELS ARE IN METRES AOD UNLESS STATED OTHERWISE.
- THIS DRAWING TO BE READ IN CONJUNCTION WITH DRAWING NUMBERS:

203742.BV.00.46.002	SITE CLEARANCE
203742.BV.00.46.003	PLAN AND LOCATION PROFILE
203742.BV.00.46.004	V-SHAPE PIER AND TYPICAL DECK CROSS SECTION
203742.BV.00.46.005	NORTH PIER
203742.BV.00.46.006	SECTIONS A-A AND B-B
203742.BV.00.46.007	SECTIONS C-C, D-D AND E-E
203742.BV.00.46.008	NORTH RAMP AND BRIDGE LEVELS
969.CRB.10	REFERENCE PLAN & HANDRAIL DETAILS
969.CRB.11	SURFACE FINISHES
969.CRB.12	RAMP FINISHES & DETAILS
969.CRB.13	PLACEMAKING DETAILS
969.CRB.14	COLOUR OPTION 1-DARK GREY
969.CRB.15	COLOUR OPTION 2-GREY/BLUE
- DIMENSIONS AND DETAILS ARE INDICATIVE AND MAY BE SUBJECT TO MINOR CHANGES DURING DEVELOPMENT OF DETAIL DESIGN.

Key
— REDLINE BOUNDARY (2700M²).

Rev	By	Chkd	Apprd	Date	Description
01	RM	BB	SP	22/08/14	Prepared for planning application



CH2MHILL

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Project
**BRISTOL CITY COUNCIL
CYCLING AMBITION FUND**

Drawing
**CAMDEN ROAD BRIDGE
LOCATION PLAN**

Drawn by: RM	Date: 22/08/14
Checked by: BB	Date: 22/08/14
Approved by: SP	Date: 22/08/14

Drawing No.	Revision
203742.BV.00.46.001	P-01

Drawing Scale: As shown at A1

Case Officer Site Photographs

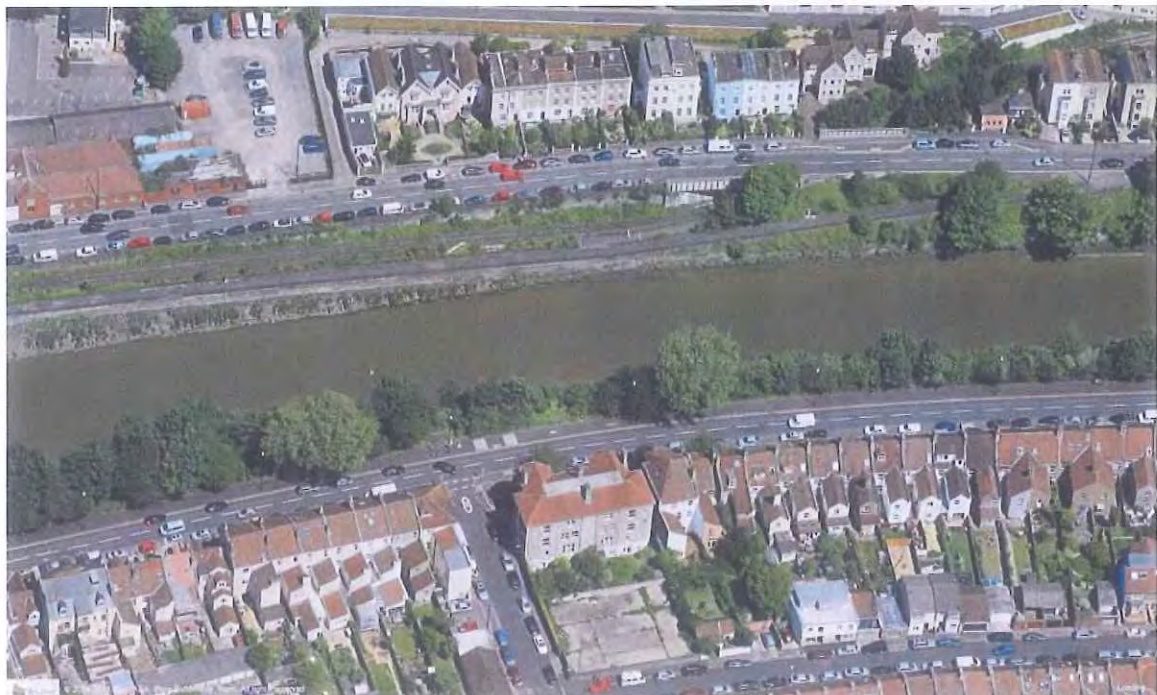
Application No:
14/04262/FB

Date of photo:
14th November 2014

Site Address: New Cut
New Bridge
Coronation Road/Cumberland
Road
Bristol



View of application site from south bank to west of the proposed landing area.



Aerial view of application site (© BlomPictometry 2012)



Path from Harbourside to Chocolate Path with Cumberland Road bridge over. Ramps/retaining structures continue under bridge.



Current connection between two paths – Variety of boundary railings/treatments and bollards can be seen.



Chocolate Path ramp to Cumberland Road – tarmac patching can be seen further along path.



View back down same ramp from opposite end – modern security fencing can be seen to right and further patching of the path.



Existing seating area and commemorative tree planted by Friend of the Avon New Cut along the ramp.



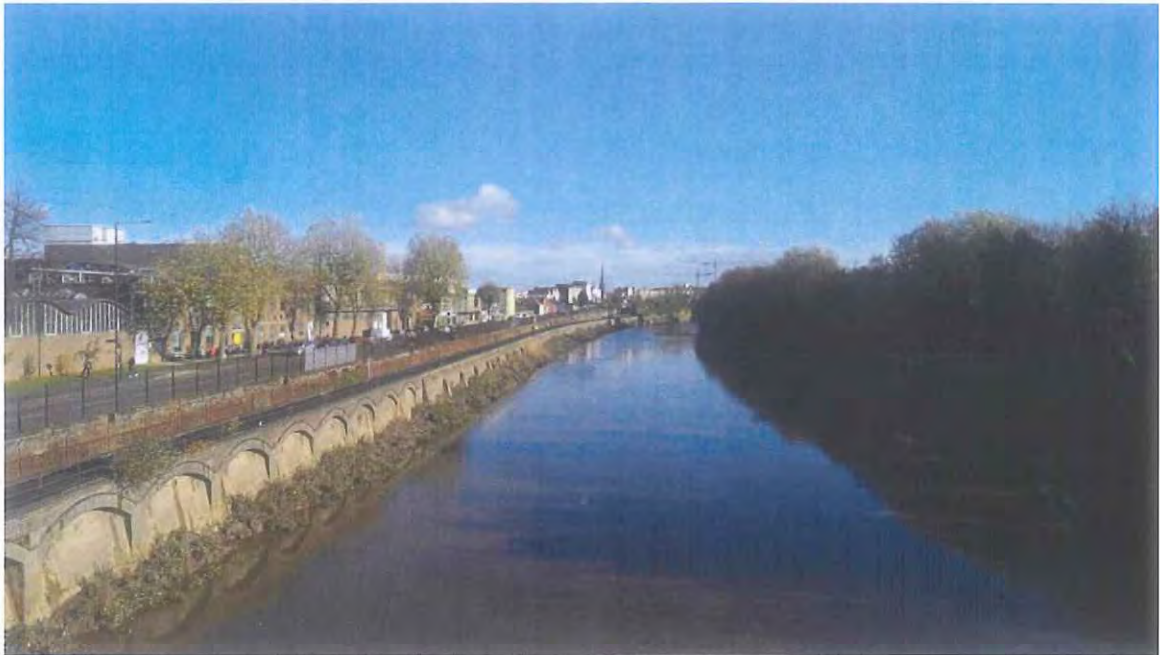
Junction of Camden Road with Coronation Road and existing tactile paving to note crossing area – where proposed bridge would meet Coronation Road.



View of boundary planting along proposed landing area to south bank.



View of south bank vegetation area to be removed.



View from Vauxhall bridge (west of proposed bridge) to application site (just to right from church spire across the river).



Example of other patchwork along chocolate path and facing materials to retaining wall of river bank (pennant stone with blue bullnose brick capping)



Existing bridge to west - Vauxhall Bridge – with raised/stepped access to north bank

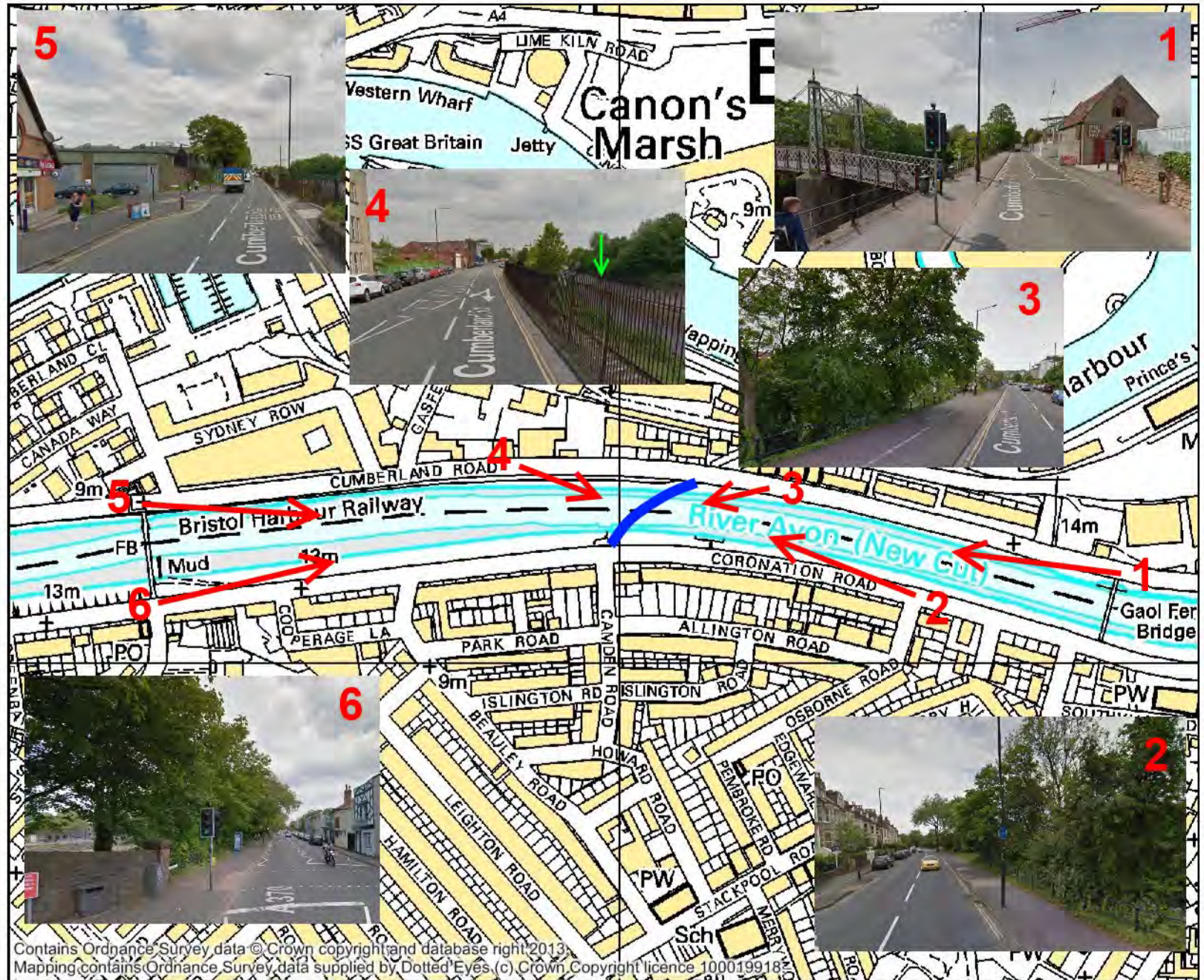


Existing bridge to east - Gaol Ferry Bridge.

Cycling Ambition Fund
Camden Road Bridge

Assessment of impacts on New Cut views

1. Looking west from the north side of Gaol Ferry Bridge. Dense vegetation at the back of the footway obscures the long view down the river
2. From the Osborne Road junction on Coronation Road looking west the vegetation here is impermeable and no view of the river is available
3. From the top of the Chocolate Path ramp looking west there is a glimpse view of the river
4. Looking east from Phidias Stone Masons there is little vegetation here and the north bank is visible. Long views are obscured by the Harbour Railway railings (green arrow indicates approximate southern bridge landing site at existing outfall)
5. From the north side of Vauxhall Bridge, the long view upstream is obscured by the Harbour Railway railings
6. From the south side of Vauxhall Bridge, the dense continuous strip of river bank vegetation obscures any view



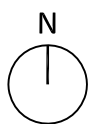
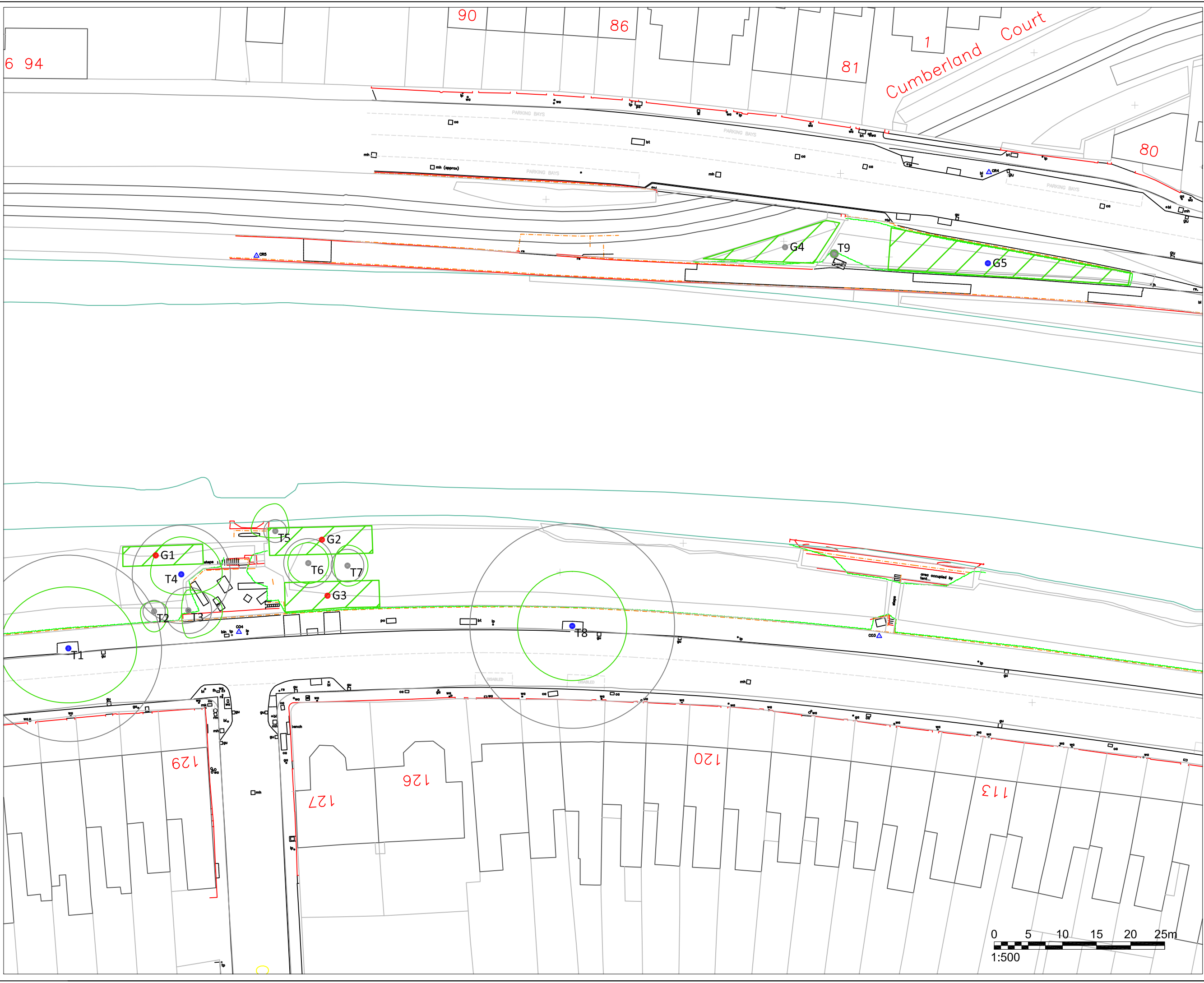
Contains Ordnance Survey data © Crown copyright and database right 2013.
Mapping contains Ordnance Survey data supplied by Dotted Eyes. (c) Crown Copyright licence 1000199182

Produced by Jon Usher
on 20 November 2014
Scale 1:5000

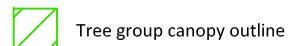
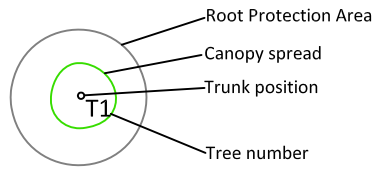


0 0.05 0.1 0.15 0.2 0.25 0.3 kilometres





Symbol Guide



BS5837:2012 - Tree Category

- **Category A Trees**
High quality and value
40+ years ULE
- **Category B Trees**
Moderate quality and value
20+ years ULE
- **Category C Trees**
Low quality and value
10+ years ULE
- **Category U Trees**
Poor quality and value
<10 years ULE



46 Huntenhull Green
 Corsley, Warminster
 BA12 7QB
 info@boskytrees.co.uk
 Tel: 01373 832778

Project Name:
**Camden Road
 Bristol**

Drawing Title:
Tree Constraints Plan

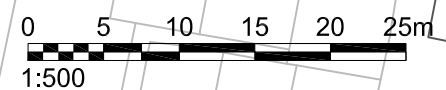
Drawing Number: TCP-1	Revision
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Client:
Bristol City Council

Agent:

Date:
14-2-2014

Scale:
1:500 @ A3



Location Plan:



NOTES

- ALL DIMENSIONS ARE IN MILLIMETRES UNLESS STATED OTHERWISE.
- ALL LEVELS ARE IN METRES AOD UNLESS STATED OTHERWISE.
- THIS DRAWING TO BE READ IN CONJUNCTION WITH DRAWING NUMBERS:

203742.BV.00.46.001	LOCATION PLAN
203742.BV.00.46.003	PLAN AND LOCATION PROFILE
203742.BV.00.46.004	V-SHAPE PIER AND TYPICAL DECK CROSS SECTION
203742.BV.00.46.005	NORTH PIER SECTIONS A-A AND B-B
203742.BV.00.46.006	SECTIONS C-C, D-D AND E-E
203742.BV.00.46.007	NORTH RAMP AND BRIDGE LEVELS
203742.BV.00.46.008	REFERENCE PLAN & HANDRAIL DETAILS
969.CRB.10	SURFACE FINISHES
969.CRB.11	RAMP FINISHES & DETAILS
969.CRB.12	PLACEMAKING DETAILS
969.CRB.13	COLOUR OPTION 1-DARK GREY
969.CRB.14	COLOUR OPTION 2-GREY/BLUE
- DIMENSIONS AND DETAILS ARE INDICATIVE AND MAY BE SUBJECT TO MINOR CHANGES DURING DEVELOPMENT OF DETAIL DESIGN.

- Key:
- VEGETATION CLEARANCE (880M²).
 - CHOCOLATE PATH TILE REMOVAL (300M²).

Rev	By	Chkd	Apprv	Date	Description
02	JEC	DC	HF	1/11/14	Existing handrail removed
01	RM	BB	SP	22/08/14	Prepared for planning application



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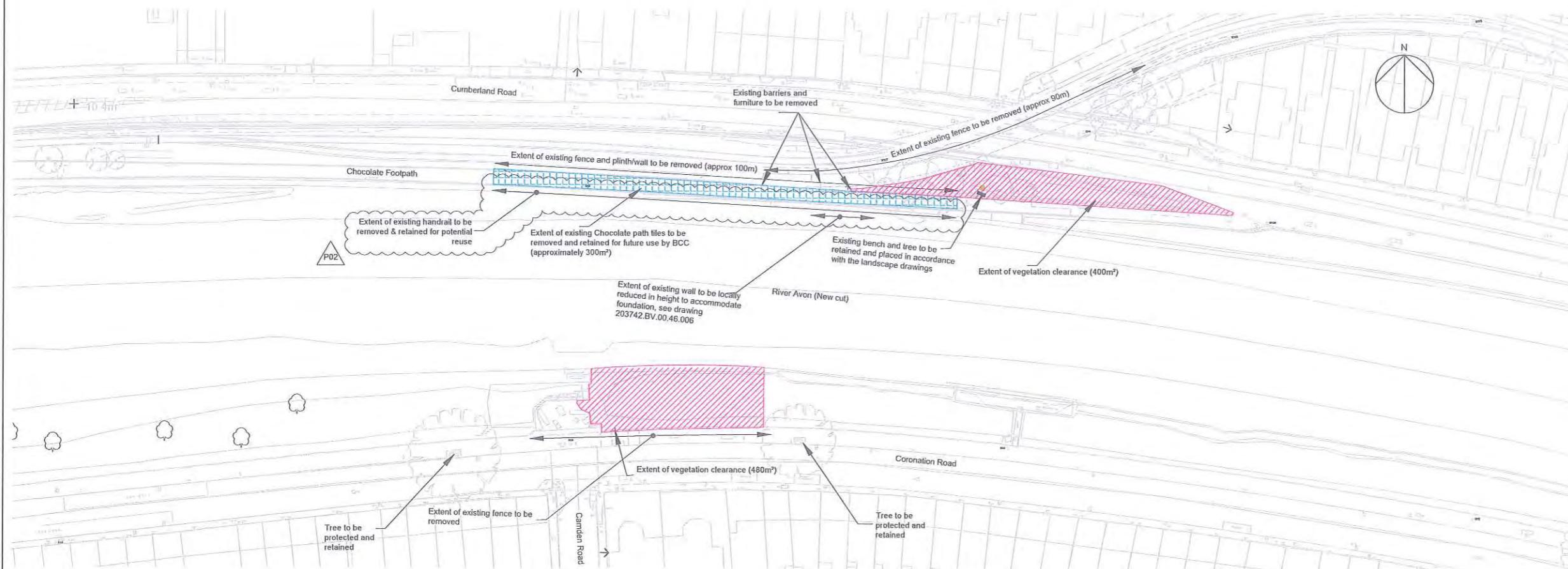
Project
**BRISTOL CITY COUNCIL
CYCLING AMBITION FUND**

Drawing
**CAMDEN ROAD BRIDGE
SITE CLEARANCE**

Drawn by: RM Date: 22/08/14
Checked by: BB Date: 22/08/14
Approved by: SP Date: 22/08/14

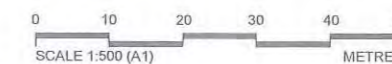
Drawing No. 203742.BV.00.46.002 Revision P 02

Drawing Scale: As shown at A1

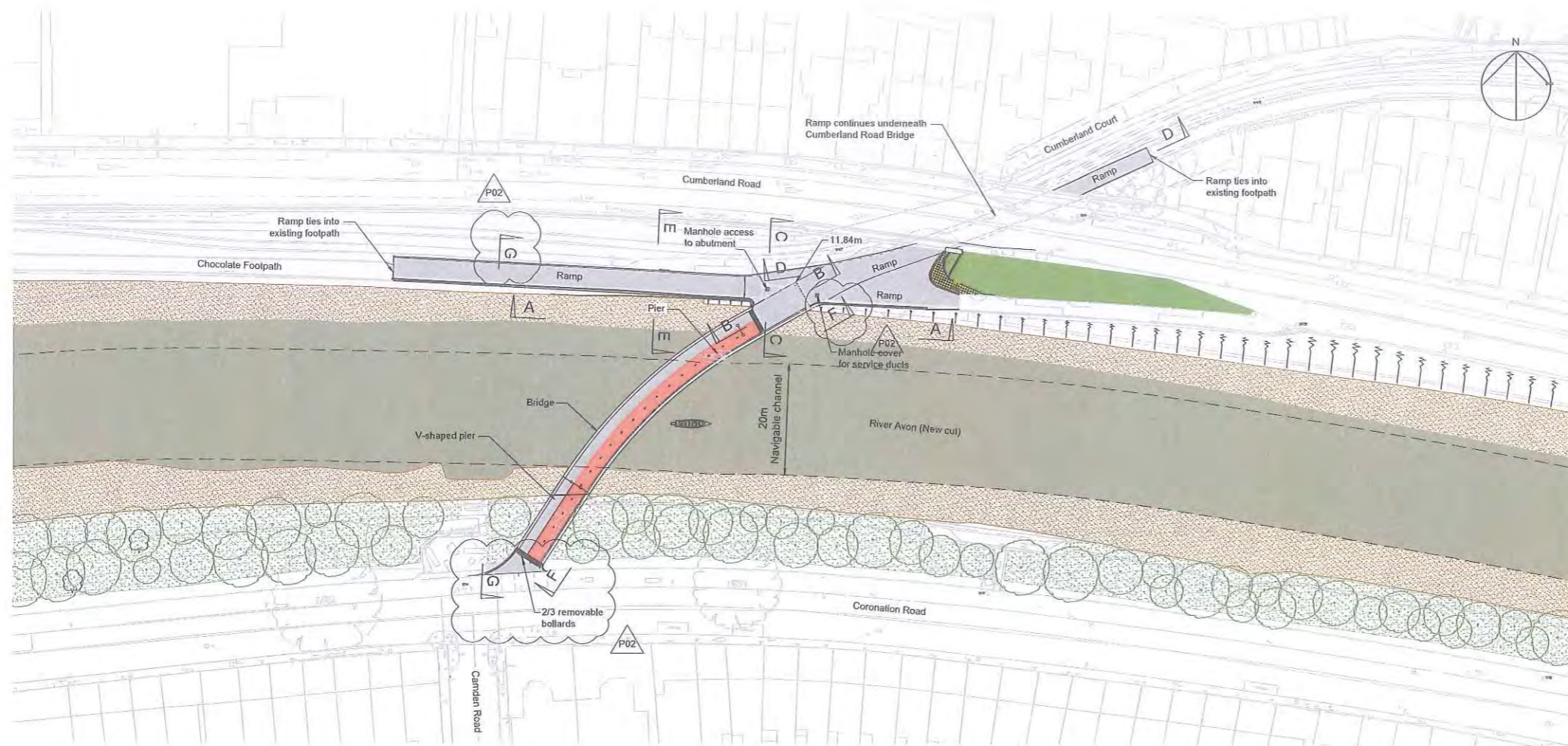


PLAN
Scale 1:500

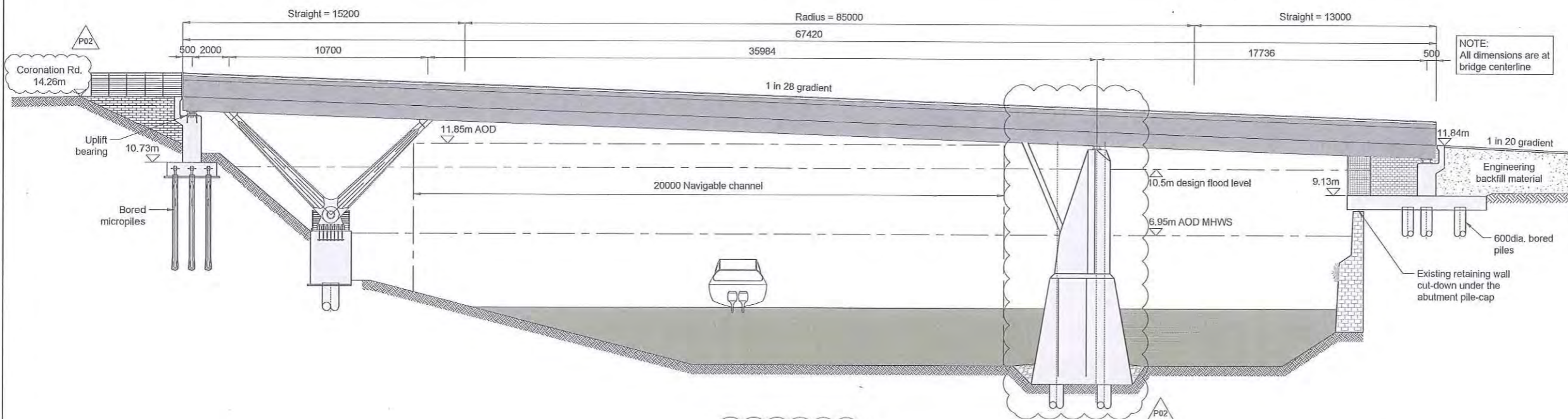
FOR PLANNING



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User and Print Date : 22/08/14 11:20:14 2/11 pm



PLAN
Scale 1:500

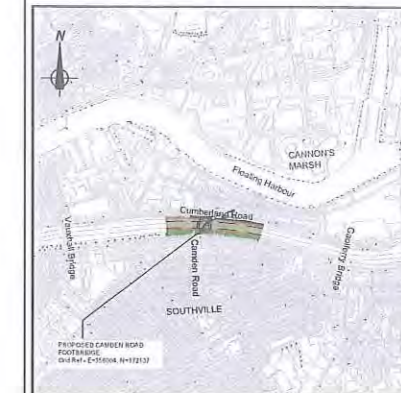


ELEVATION F-F
SCALE 1:125

FOR PLANNING



Location Plan:



NOTES

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- THIS DRAWING TO BE READ IN CONJUNCTION WITH DRAWING NUMBERS:

203742.BV.00.46.001	LOCATION PLAN
203742.BV.00.46.002	SITE CLEARANCE
203742.BV.00.46.004	V-SHAPE PIER AND TYPICAL DECK CROSS SECTION
203742.BV.00.46.005	NORTH PIER
203742.BV.00.46.006	SECTIONS A-A AND B-B
203742.BV.00.46.007	SECTIONS C-C, D-D AND E-E
203742.BV.00.46.008	NORTH RAMP AND BRIDGE LEVELS
203742.BV.00.46.009	SECTION G-G
969.CRB-10	REFERENCE PLAN & HANDRAIL DETAILS
969.CRB-11	SURFACE FINISHES
969.CRB-12	RAMP FINISHES & DETAILS
969.CRB-13	PLACEMAKING DETAILS
969.CRB-14	COLOUR OPTION 1-DARK GREY
969.CRB-15	COLOUR OPTION 2-GREY/BLUE
- DIMENSIONS AND DETAILS ARE INDICATIVE AND MAY BE SUBJECT TO MINOR CHANGES DURING DEVELOPMENT OF DETAIL DESIGN.

Rev	By	Chkd	Appvd	Date	Description
02	DC	DC	NIF	25/11/14	Elevation amended, elevation G-G added
01	RM	BB	SP	22/09/14	Prepared for planning application



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Project
**BRISTOL CITY COUNCIL
CYCLING AMBITION FUND**

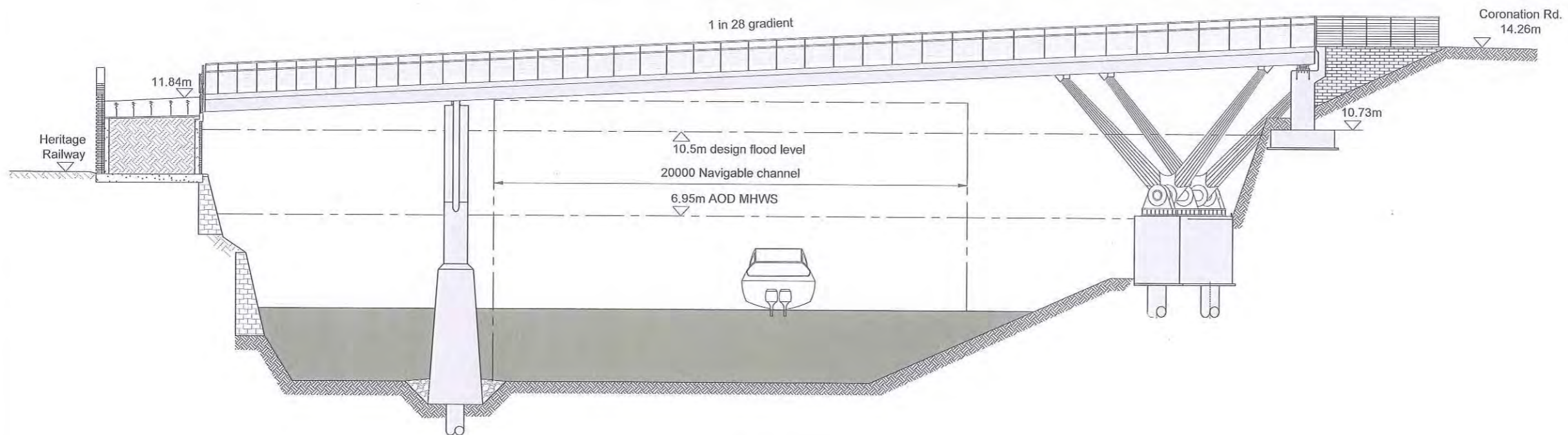
Drawing
**CAMDEN ROAD BRIDGE
PLAN AND
LONGITUDINAL PROFILE**

Drawn by: RM	Date: 22/09/14
Checked by: BB	Date: 22/09/14
Approved by: SP	Date: 22/09/14

Drawing No.	Revision
203742.BV.00.46.003	P 02

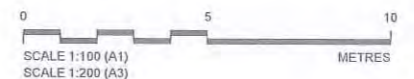
Drawing Scale: As shown at A1

Drawing No. 203742.BV.00.46.003
 Project: BRISTOL CITY COUNCIL CYCLING AMBITION FUND
 Drawing Title: CAMDEN ROAD BRIDGE PLAN AND LONGITUDINAL PROFILE
 Date: 22/09/14
 Scale: 1:500 (A1)
 User and Print Date: 22/09/14 12:11 pm

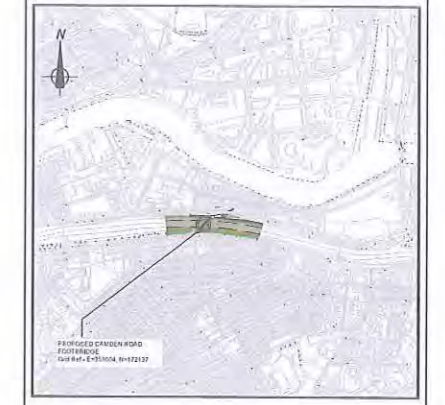


ELEVATION G-G
SCALE 1:100

FOR PLANNING



Location Plan:




NOTES

1. ALL DIMENSIONS ARE IN MILLIMETRES UNLESS STATED OTHERWISE.
2. ALL LEVELS ARE IN METRES AOD UNLESS STATED OTHERWISE.
3. THIS DRAWING TO BE READ IN CONJUNCTION WITH DRAWING NUMBERS:

203742.BV.00.46.001 203742.BV.00.46.002 203742.BV.00.46.003 203742.BV.00.46.004 203742.BV.00.46.005 203742.BV.00.46.006 203742.BV.00.46.007 203742.BV.00.46.008 969.CRB.10 969.CRB.11 969.CRB.12 969.CRB.13 969.CRB.14 GREY 969.CRB.15 2-GREY/BLUE	LOCATION PLAN SITE CLEARANCE PLAN AND ELEVATION V-SHAPE PIER AND TYPICAL DECK CROSS SECTION NORTH PIER SECTIONS A-A AND B-B SECTIONS C-C, D-D AND E-E NORTH RAMP AND BRIDGE LEVELS REFERENCE PLAN & SURFACE FINISHES & RAMP FINISHES & DETAILS PLACEMAKING DETAILS COLOUR OPTION 1-DARK GREY COLOUR OPTION 2-GREY/BLUE
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4. DIMENSIONS AND DETAILS ARE INDICATIVE AND MAY BE SUBJECT TO MINOR CHANGES DURING DEVELOPMENT OF DETAIL DESIGN.

Rev	By	Chkd	Apprd	Date	Description
01	JRV	DJC	NAF	21/11/14	Prepared for planning application

Client



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Project
**BRISTOL CITY COUNCIL
 CYCLING AMBITION FUND**

Drawing
**CAMDEN ROAD BRIDGE
 ELEVATION G-G**

Drawn by: JRV	Date: 21/11/14
Checked by: -	Date: -
Approved by: -	Date: -

Drawing No. 203742.BV.00.46.009	Revision P 01
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Drawing Scale: As shown at A1

Drawing file path: \\server\projects\203742\203742_BV_00_46_009_Elevation G-G.dwg
 User and Plot Date: 21/11/2014 2:11 pm
 Project: Bristol City Council - Camden Road Cycleway
 Project Path: \\server\projects\203742\203742_BV_00_46_009_Elevation G-G.dwg
 Project Name: Camden Road Cycleway
 Project Number: 203742



Cycling Ambition
for Bristol City Council

**CAMDEN ROAD BRIDGE
COLOUR OPTION I**

Dark Grey- 7022
Red Oxide - 3009

August 2014

969.CRB.14

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CUMBERLAND ROAD

Ramp

Ramp

Ramp

Ramp

Existing ramp

Pedestrian and shared area surfaces, 5mm thick epoxy bonded mineral coated surface (Cicol NT Slurry or similar) Colour - 'bauxite'

2-3 removable bollards equally spaced at <1800mm centres to prevent vehicular access onto the bridge

Directional GRP corduroy tactile paving, 800mm wide

Cycle lane, 5mm thick epoxy bonded mineral coated surface (Cicol NT Slurry or similar) Colour - 'red granite'

White line in centre of cycle lane, 50 x 300mm at 3000mm centres

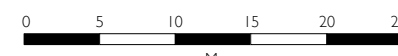
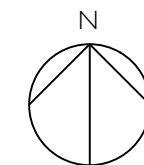
Existing tarmac area replaced with salvaged 'chocolate block' paviours.

THE NEW CUT

Directional GRP corduroy tactile paving, 800mm wide

CORONATION ROAD

CAMDEN ROAD



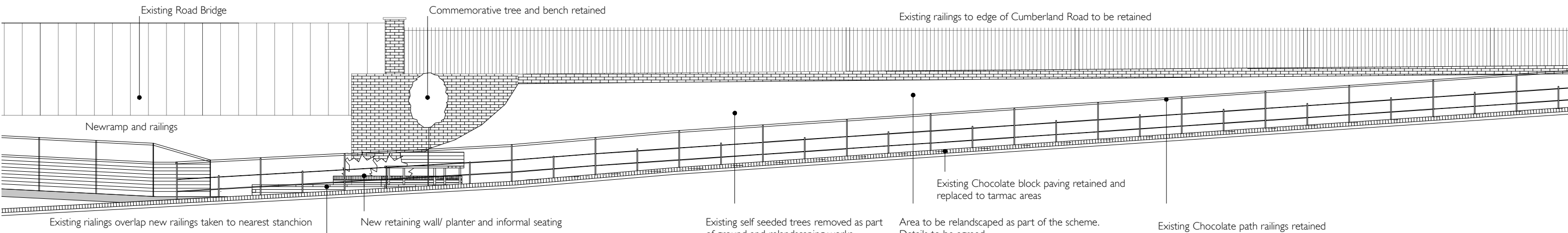
Cycling Ambition for Bristol City Council

CAMDEN ROAD BRIDGE SURFACE FINISHES

1:500 at A3 August 2014

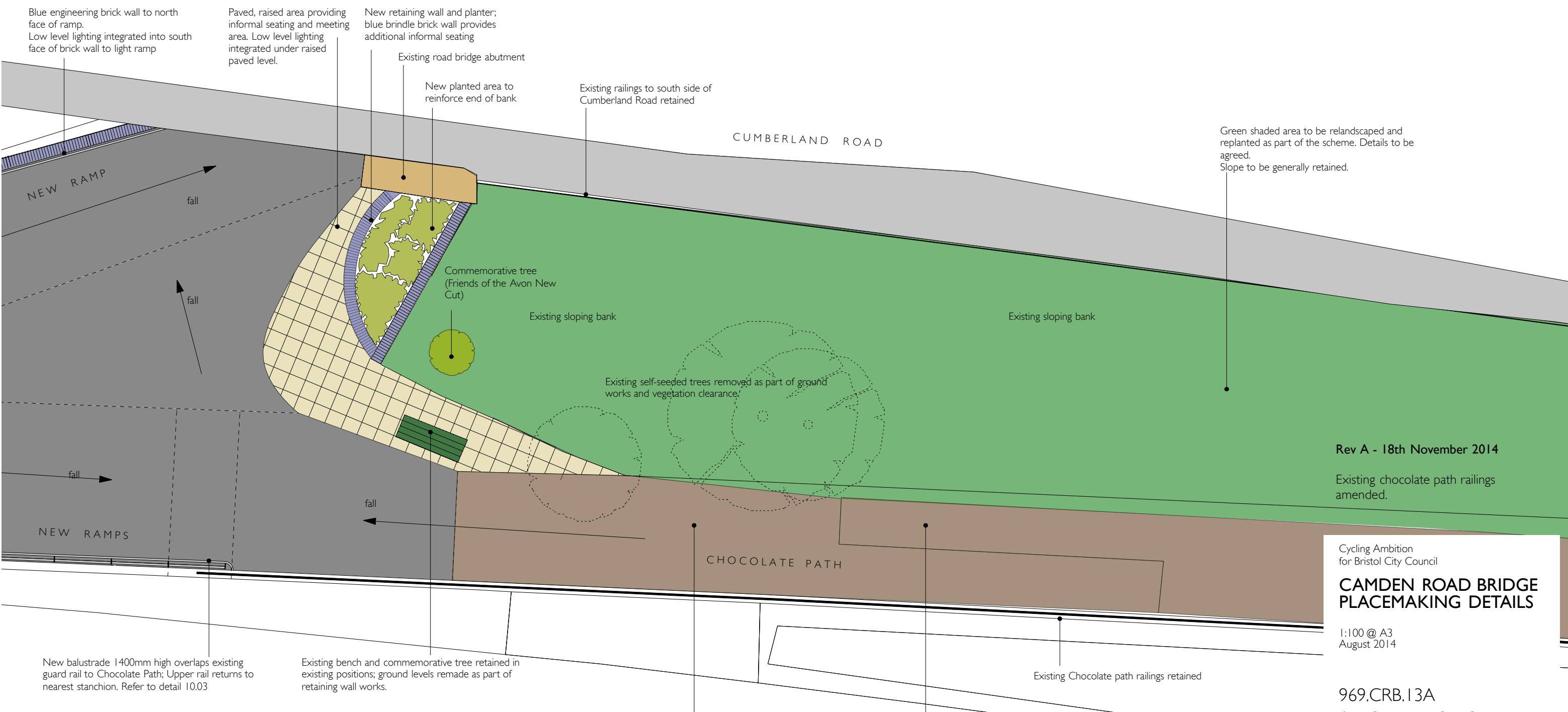
969.CRB.11

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13.01 CHOCOLATE PATH ELEVATION LOOKING NORTH

Scale 1:100 at A3



13.02 CHOCOLATE PATH & NEW RAMP

Scale 1:100 at A3

Rev A - 18th November 2014

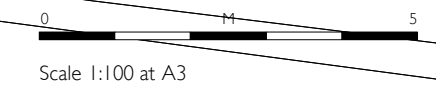
Existing chocolate path railings amended.

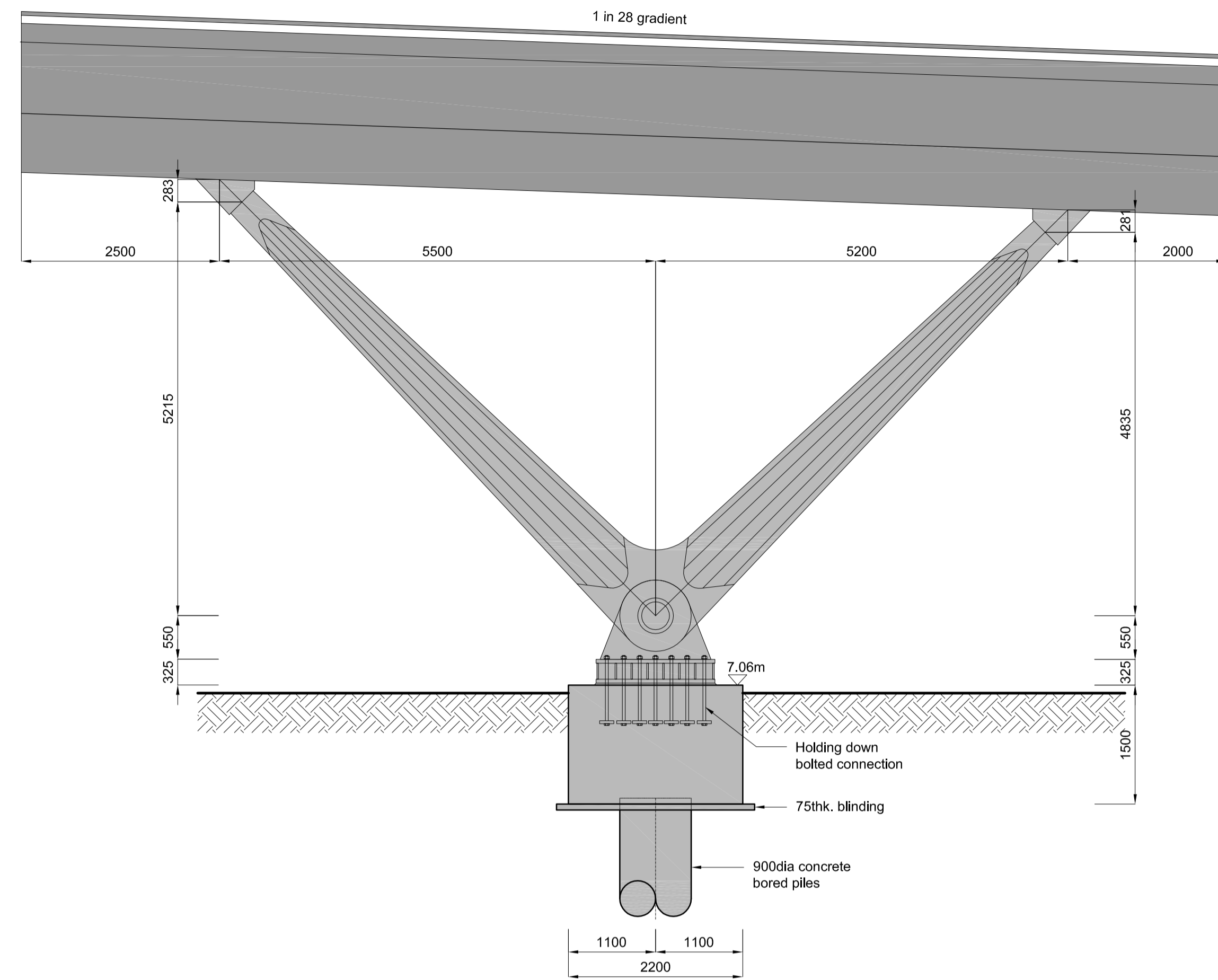
Cycling Ambition for Bristol City Council

CAMDEN ROAD BRIDGE PLACEMAKING DETAILS

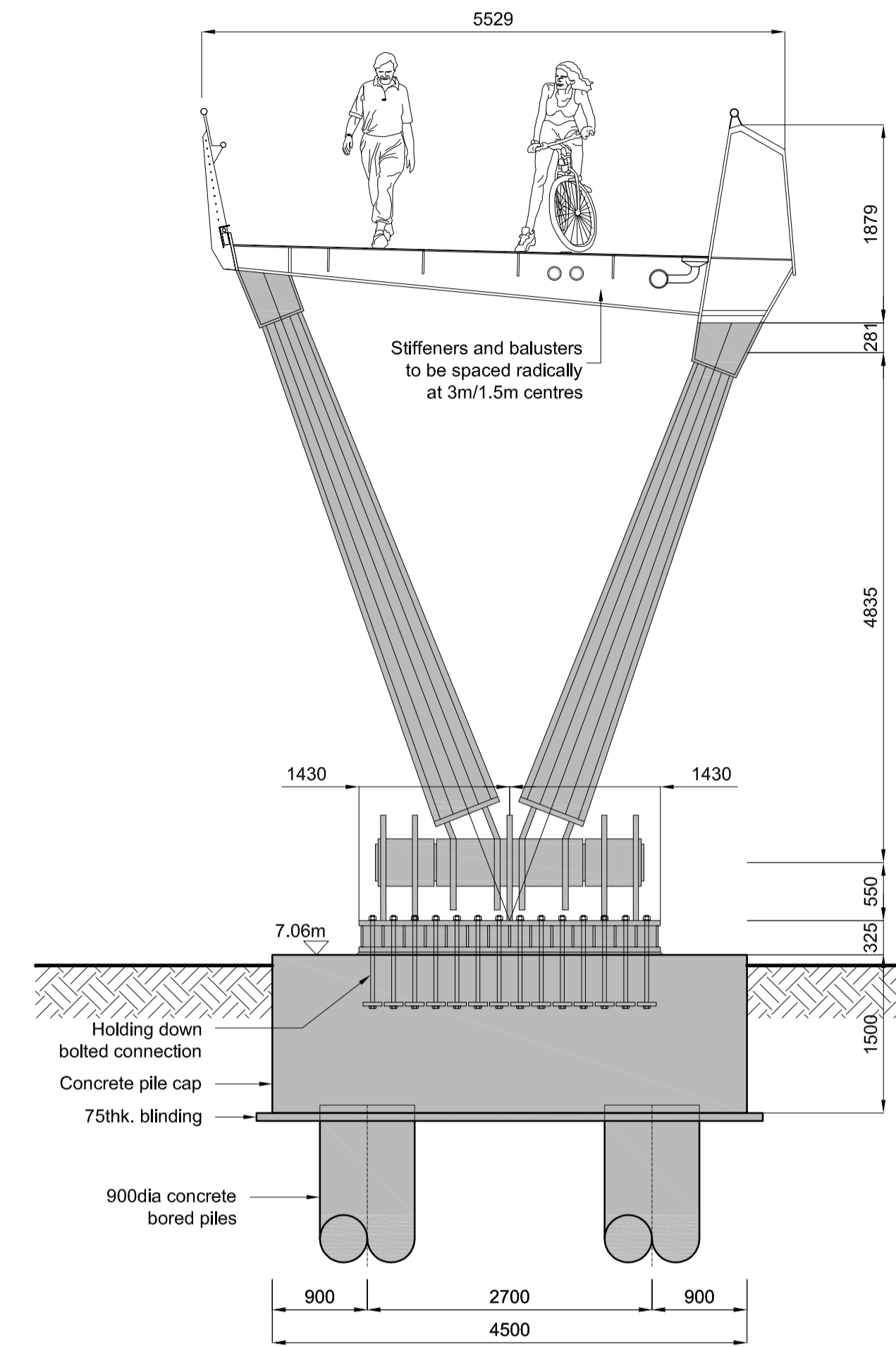
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969.CRB.13A
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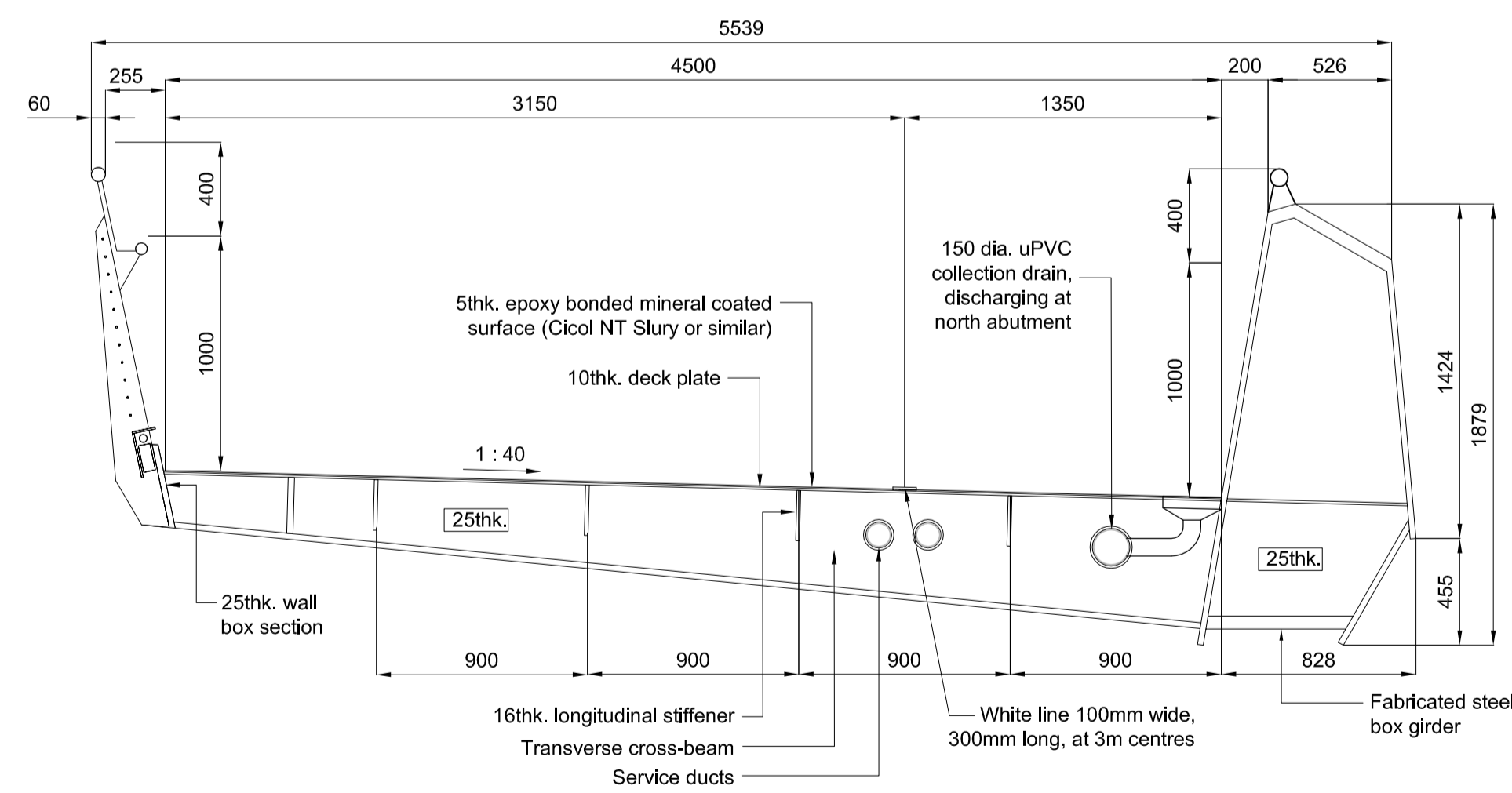




V-PIER ELEVATION
SCALE 1:50

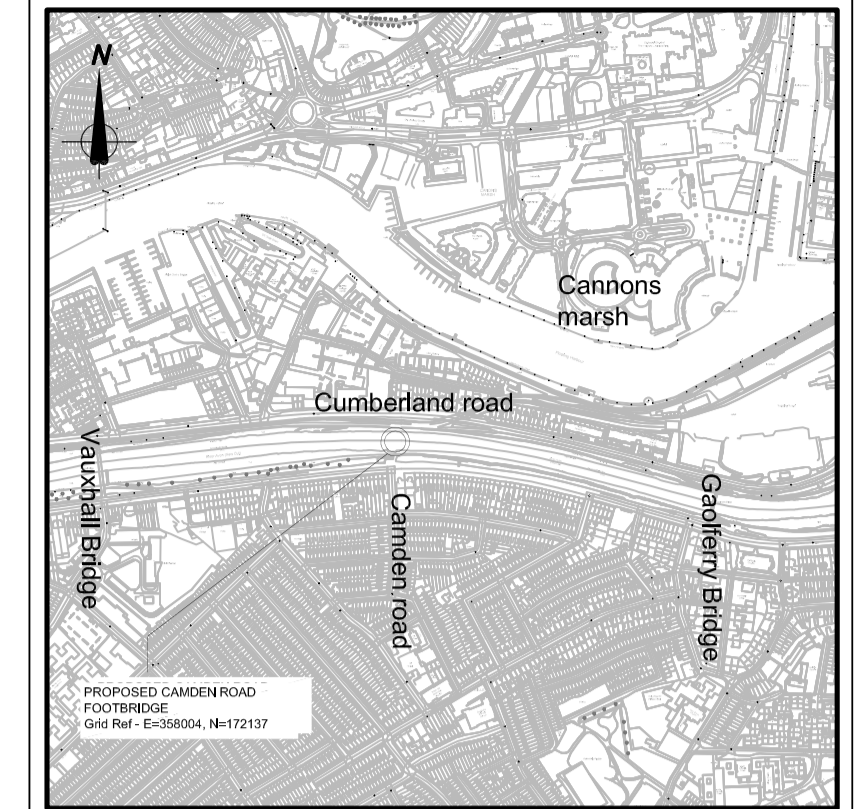


CROSS SECTION AT V-PIER
SCALE 1:50



TYPICAL CROSS SECTION THROUGH FOOTBRIDGE DECK
SCALE 1:25

Location Plan:



NOTES

- ALL DIMENSIONS ARE IN MILLIMETRES UNLESS STATED OTHERWISE.
- ALL LEVELS ARE IN METRES AOD UNLESS STATED OTHERWISE.
- FOR LOCATION OF V-PIER REFER TO DRAWING NUMBER CRB-003.
- DIMENSIONS AND DETAILS ARE INDICATIVE AND MAY BE SUBJECT TO MINOR CHANGES DURING DEVELOPMENT OF DETAIL DESIGN.
- THIS DRAWING TO BE READ IN CONJUNCTION WITH DRAWING NUMBERS:

203742.BV.00.46.001	LOCATION PLAN
203742.BV.00.46.002	SITE CLEARANCE
203742.BV.00.46.003	PLAN AND LOCATION PROFILE
203742.BV.00.46.005	NORTH PIER
203742.BV.00.46.006	SECTIONS A-A AND B-B
203742.BV.00.46.007	SECTIONS C-C, D-D AND E-E
203742.BV.00.46.008	NORTH RAMP AND BRIDGE LEVELS
969.CRB.10	REFERENCE PLAN & HANDRAIL DETAILS
969.CRB.11	SURFACE FINISHES
969.CRB.12	RAMP FINISHES & DETAILS
969.CRB.13	PLACEMAKING DETAILS
969.CRB.14	COLOUR OPTION 1-DARK GREY
969.CRB.15	COLOUR OPTION 2-GREY/BLUE

Rev	By	Chkd	Apprvd	Date	Description
01	RM	BB	SP	22/08/14	Prepared for planning application



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Project
**BRISTOL CITY COUNCIL
CYCLING AMBITION FUND**

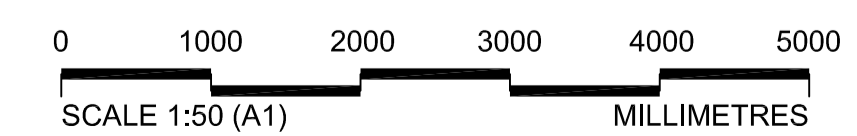
Drawing
**CAMDEN ROAD BRIDGE
V-SHAPE PIER AND
TYPICAL DECK CROSS SECTION**

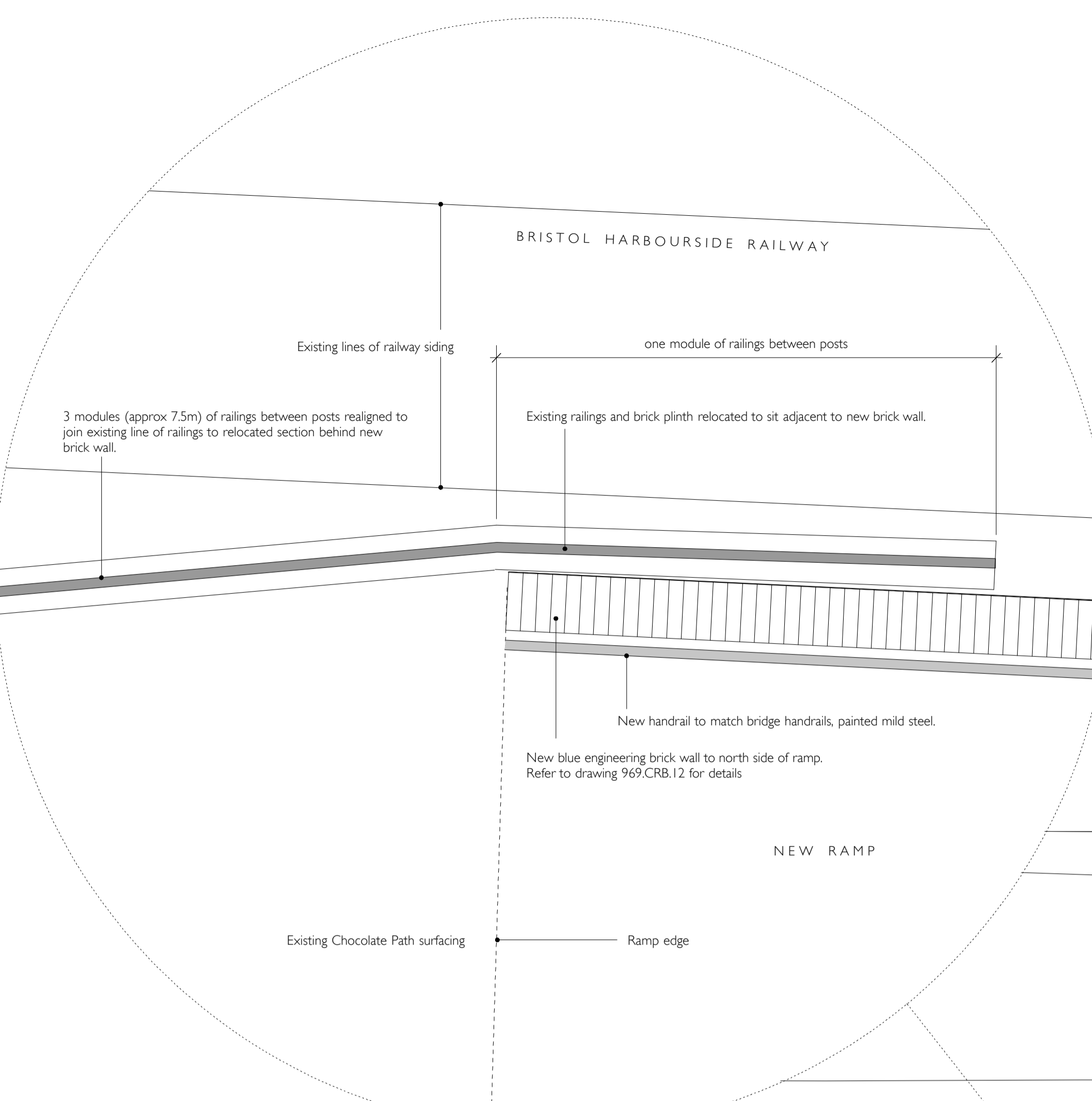
Drawn by: RM	Date: 22/08/14
Checked by: BB	Date: 22/08/14
Approved by: SP	Date: 22/08/14

Drawing No.	Revision
203742.BV.00.46.004	P-01

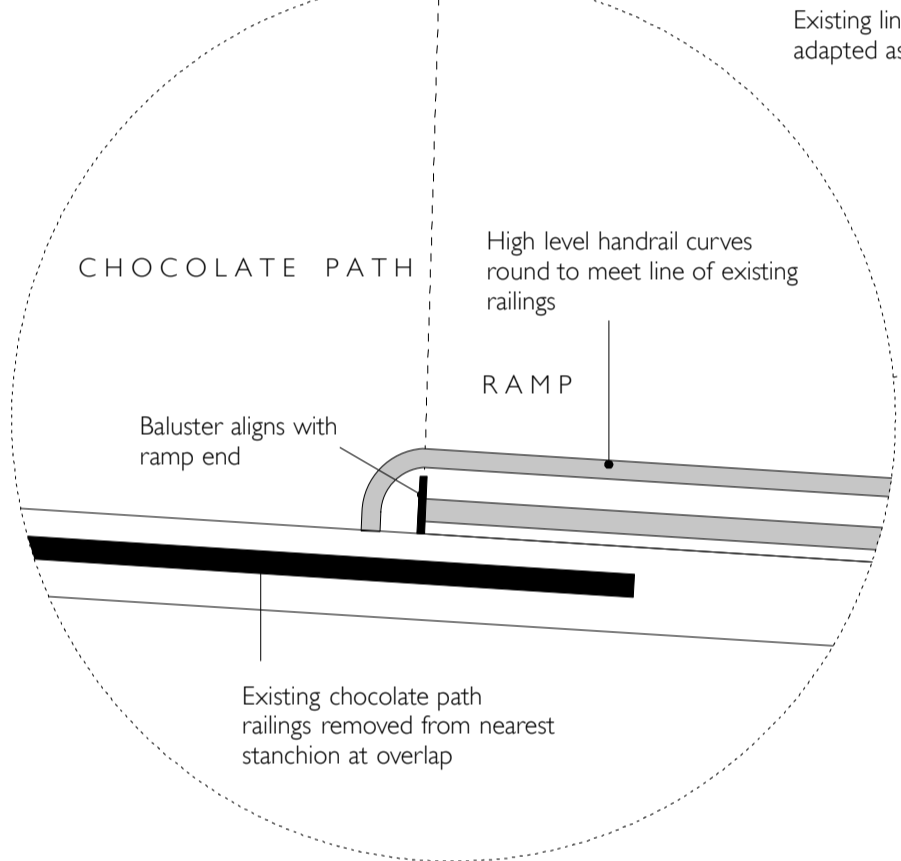
Drawing Scale: As shown at A1

FOR PLANNING

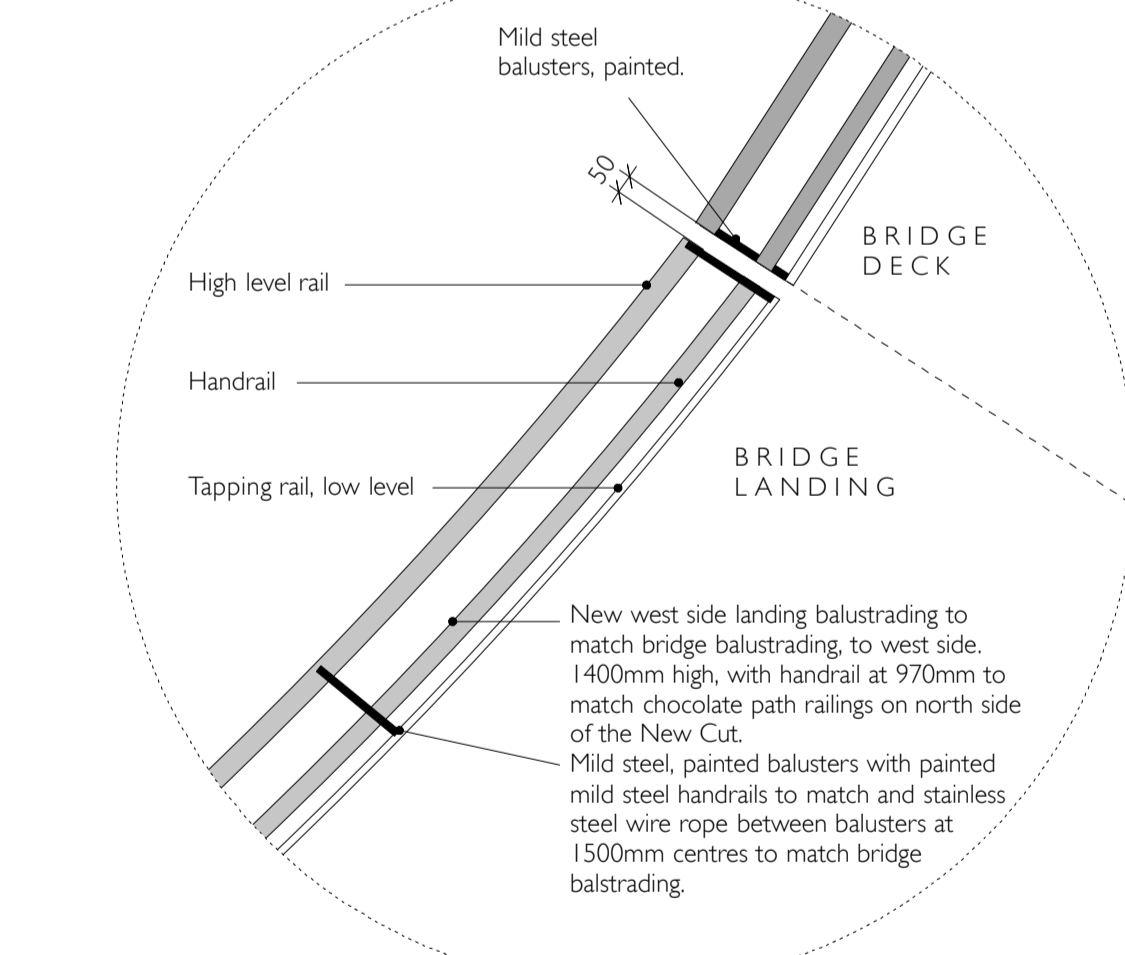




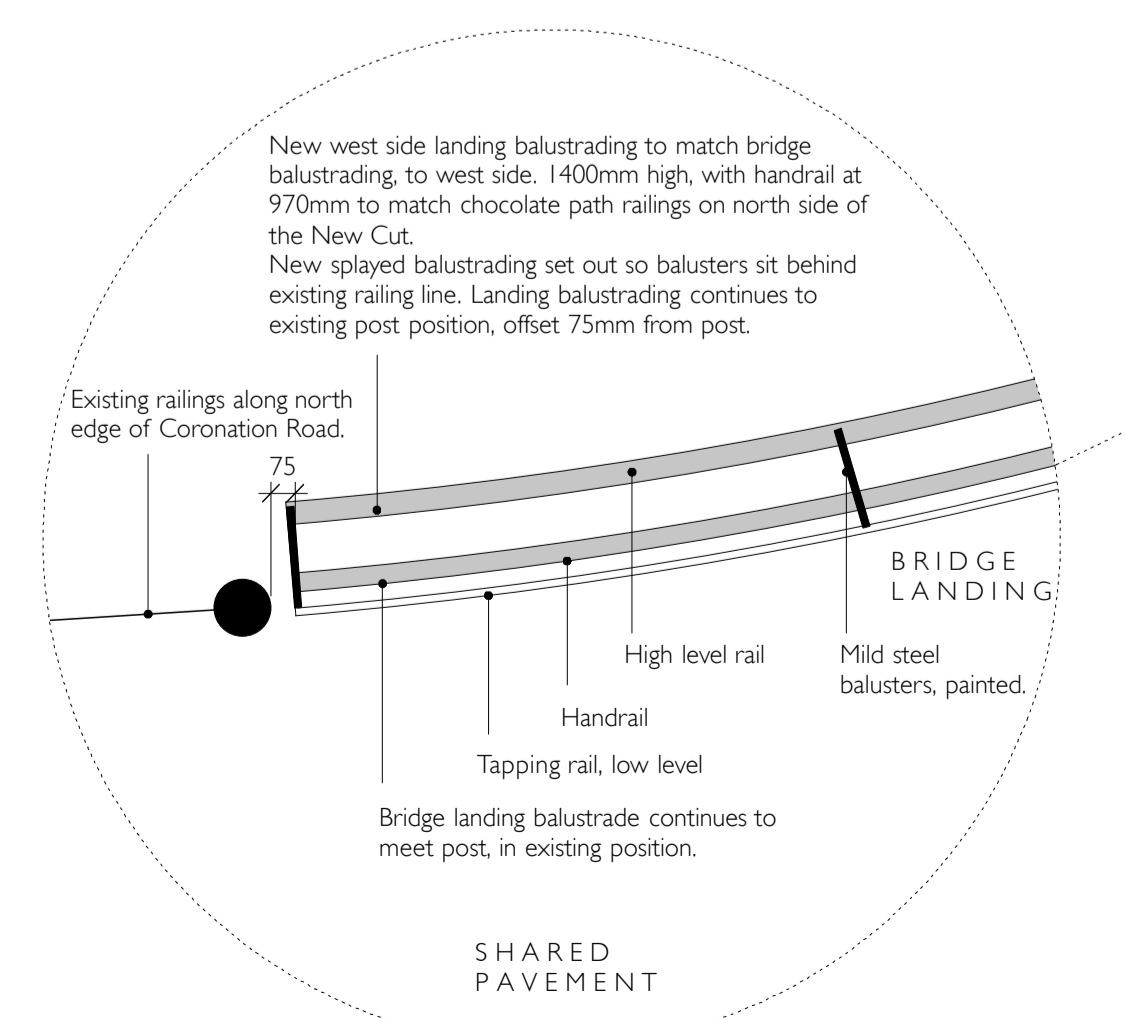
10.01 WEST RAMP ENTRANCE
Junction between new ramp wall and existing railway railings.
Scale: 1:20 at A0



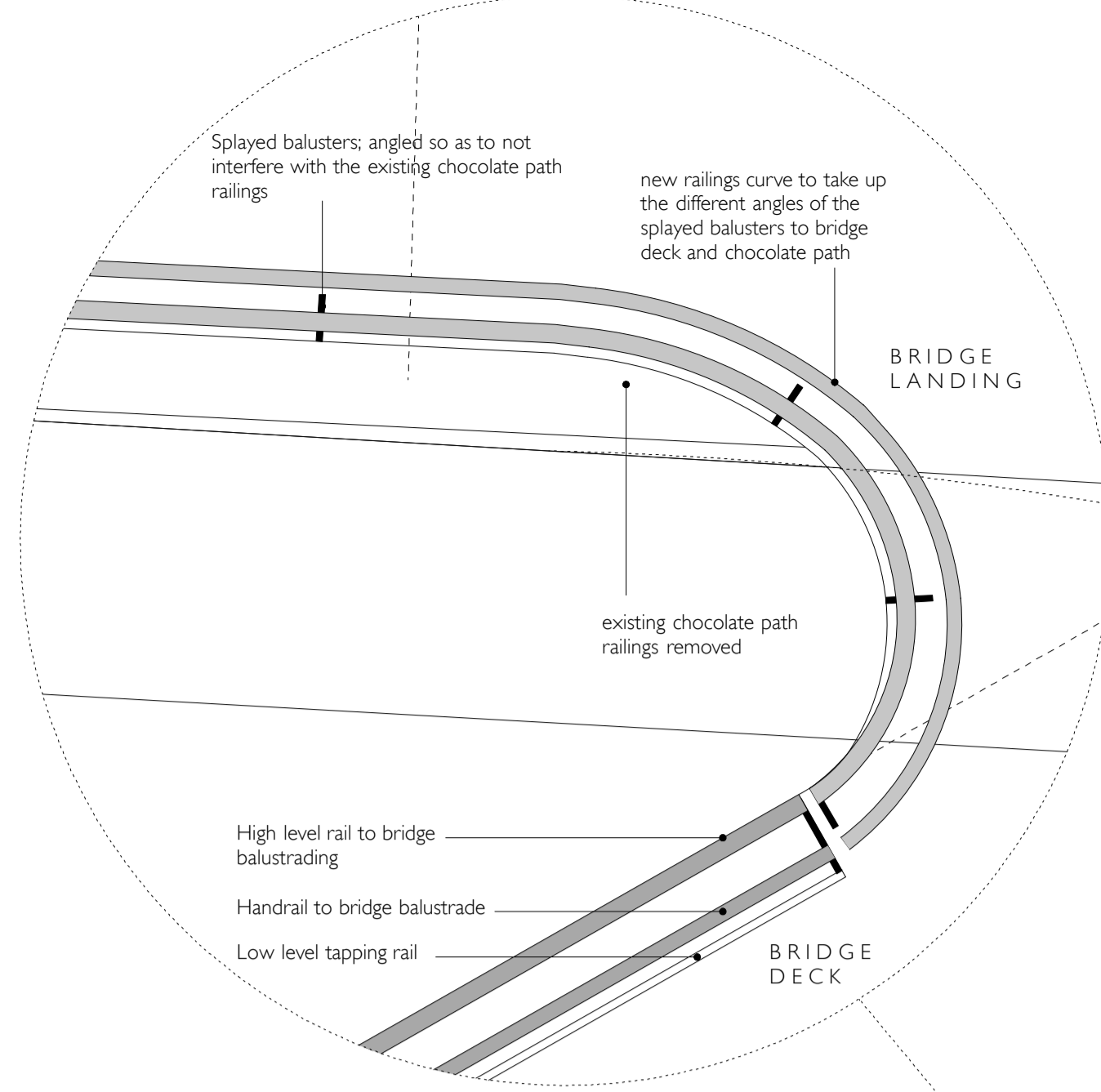
10.02 WEST RAMP ENTRANCE
Junction between new ramp wall and existing chocolate path railings.
Scale: 1:20 at A0



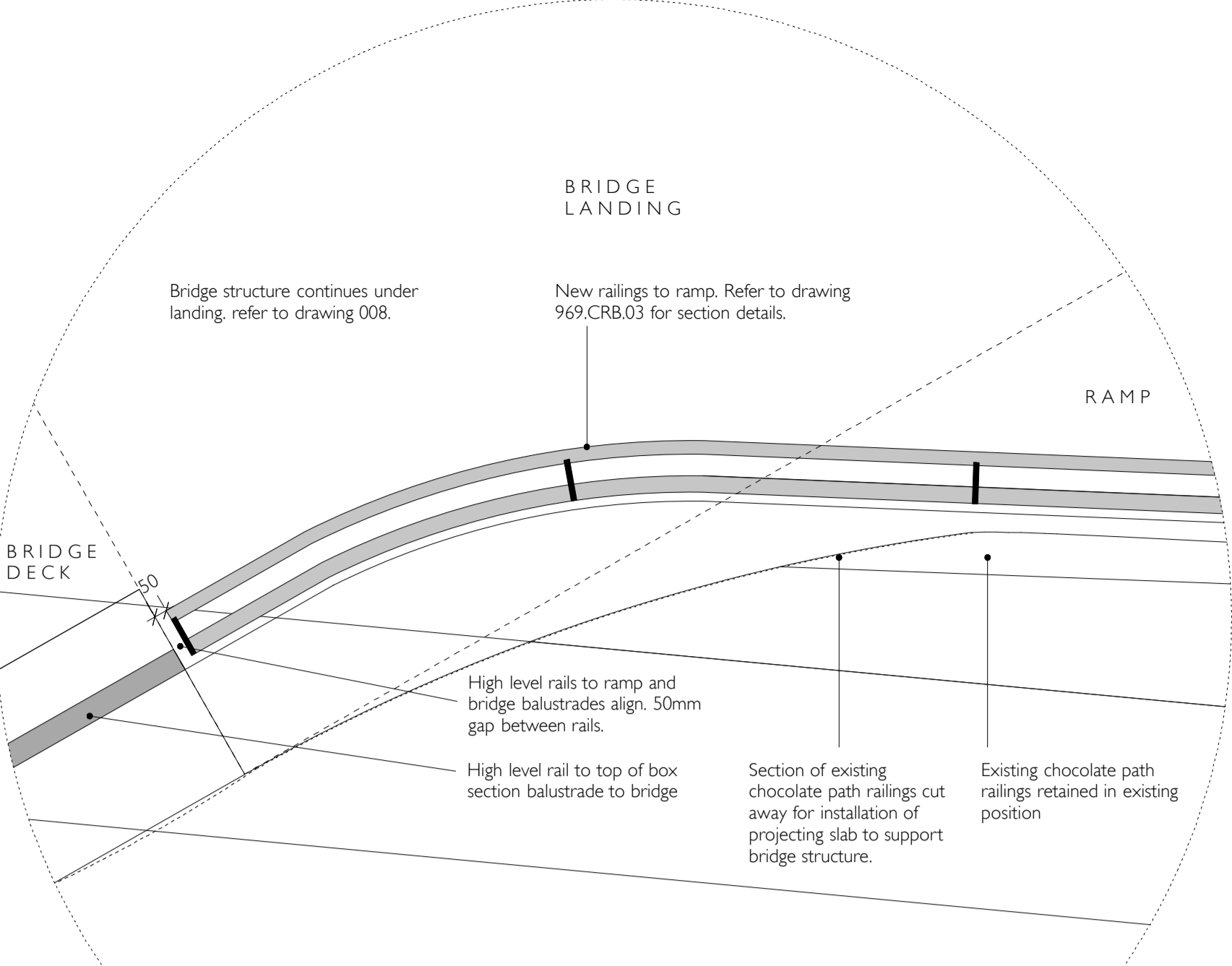
10.12 SOUTH WEST ENTRANCE
Junction between new balustrade and bridge structure.
Scale: 1:20



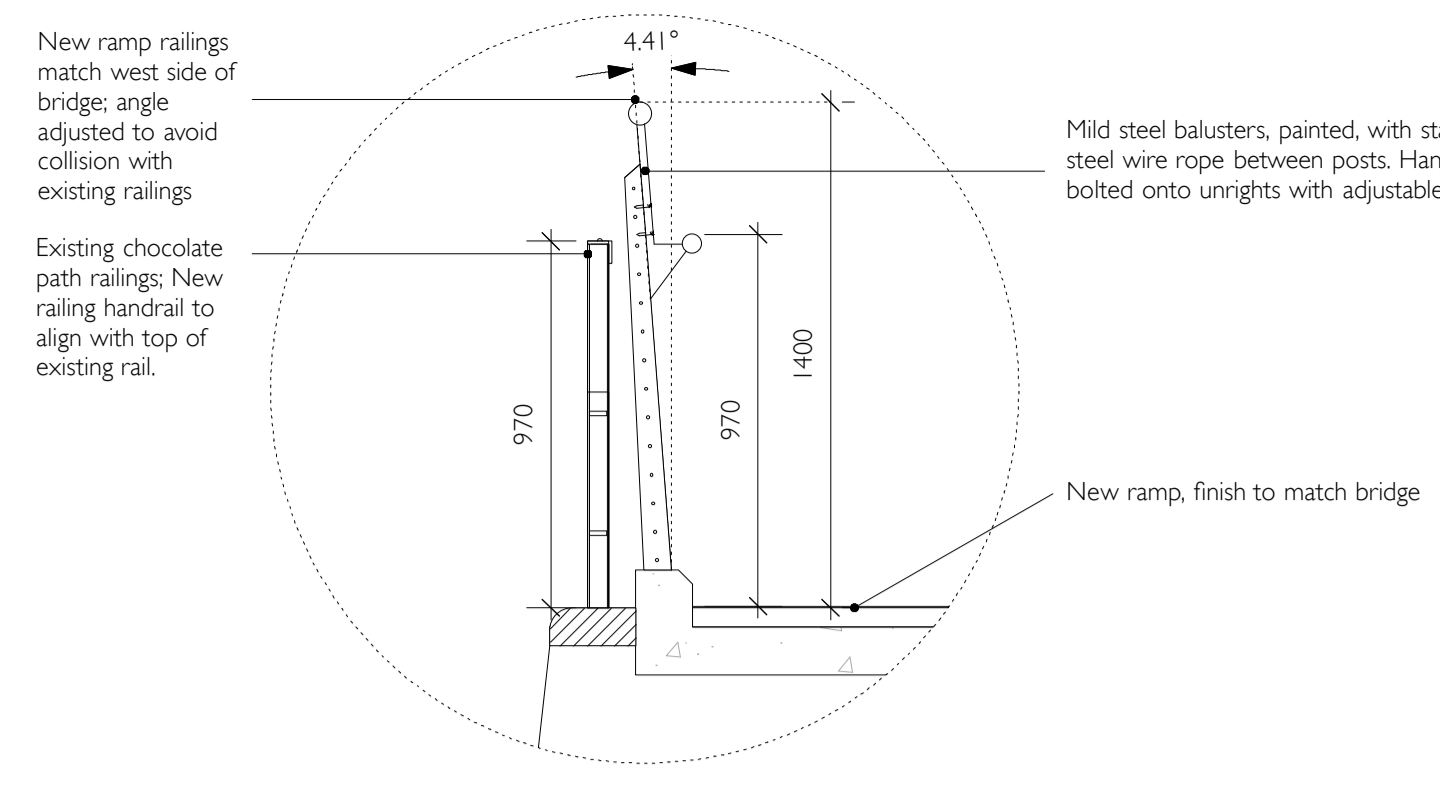
10.11 SOUTH WEST ENTRANCE
Junction between existing guarding and new balustrade.
Scale: 1:20



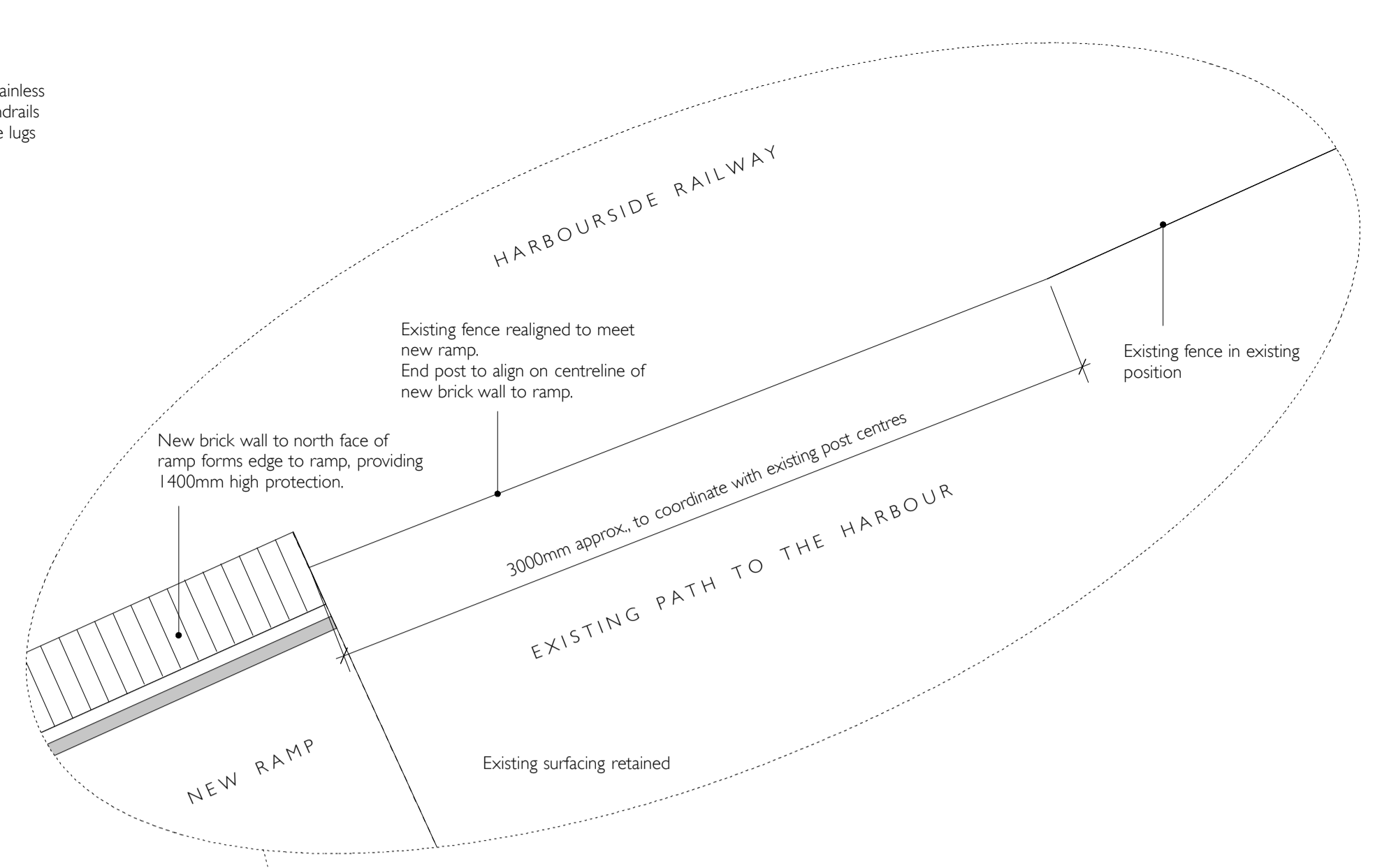
10.03 NORTH WEST ENTRANCE
Junction between new balustrade, existing guarding and new bridge.
Scale: 1:20



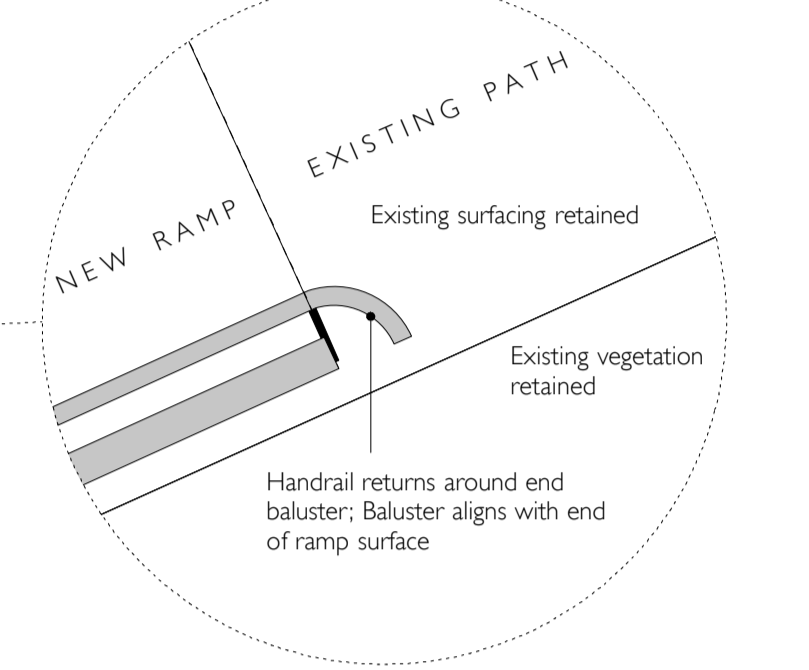
10.04 NORTH EAST ENTRANCE
Junction between new balustrade, existing guarding and new bridge.
Scale: 1:20



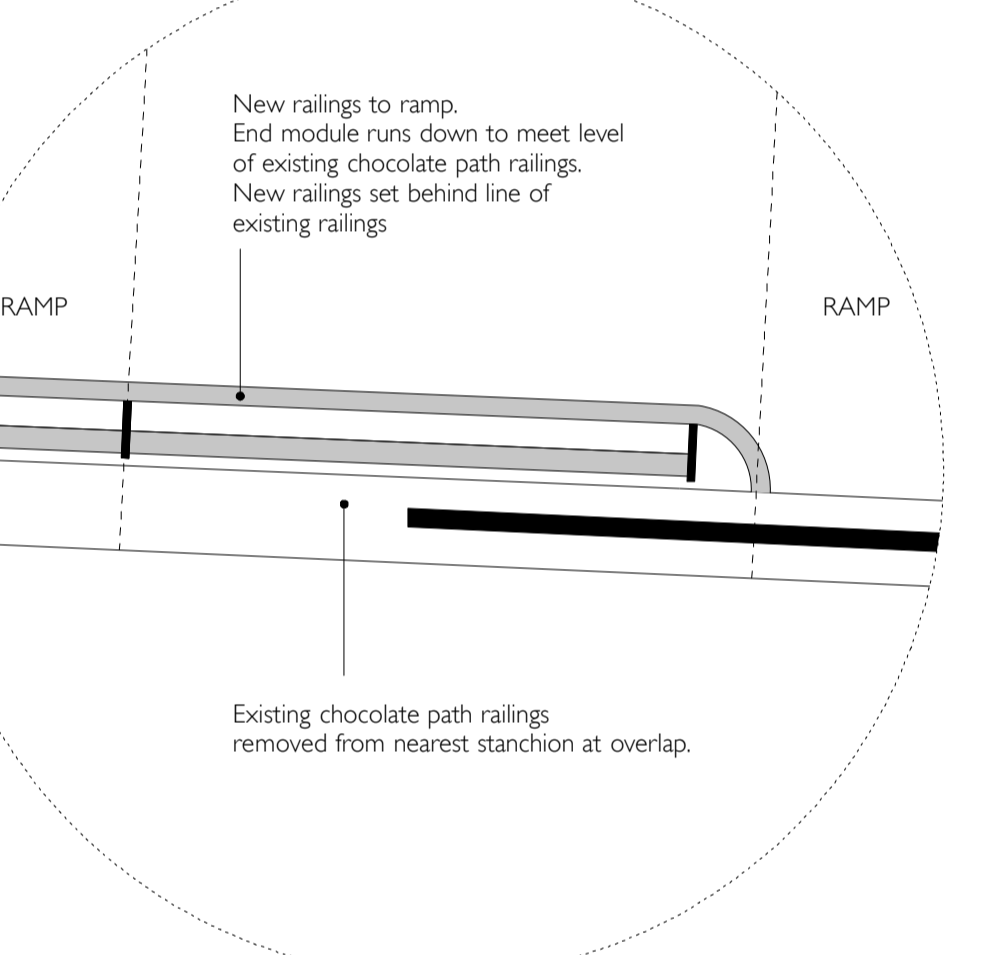
10.05 RAMP & CHOCOLATE PATH RAILINGS
Junction between new brick wall to ramp and existing fence to railway.
Scale: 1:20



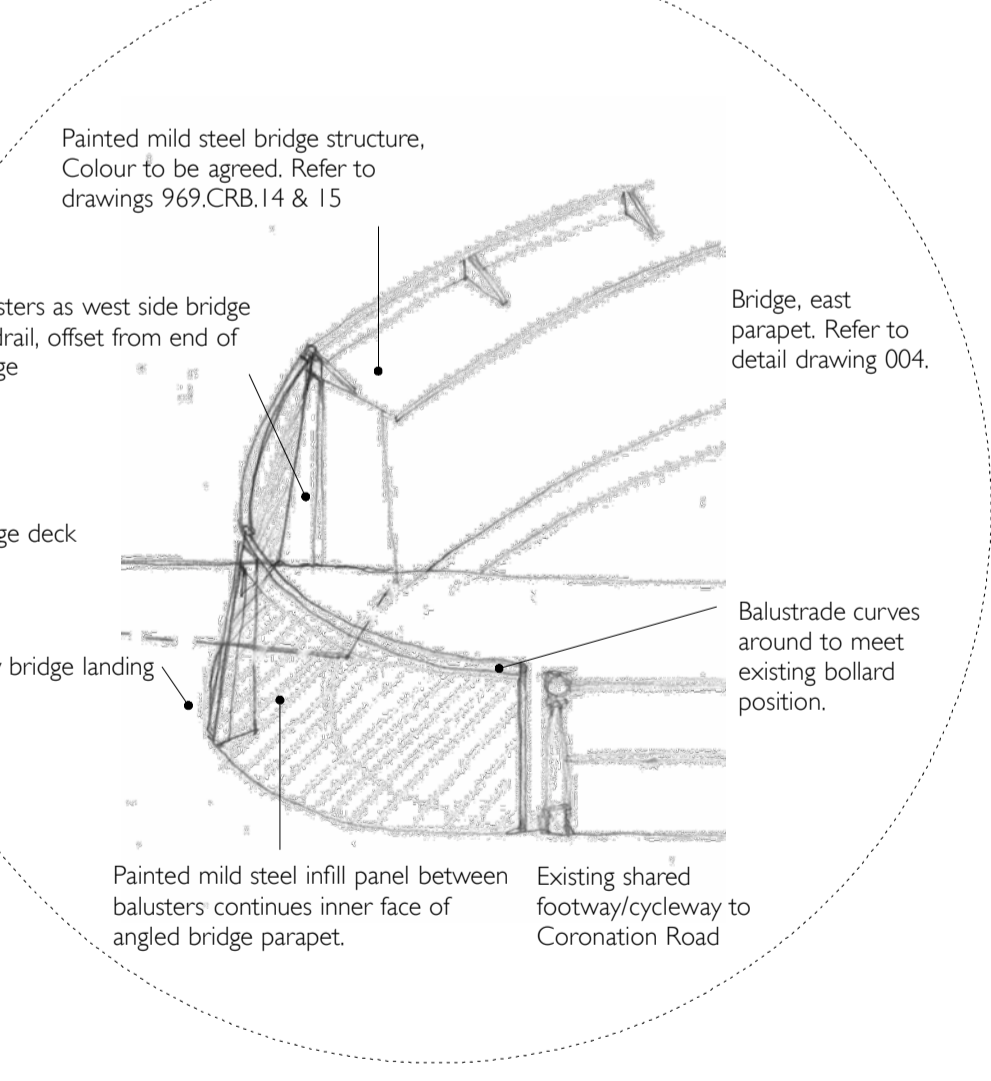
10.06 NORTH EAST END OF RAMP
Junction between new brick wall to ramp and existing fence to railway.
Scale: 1:20



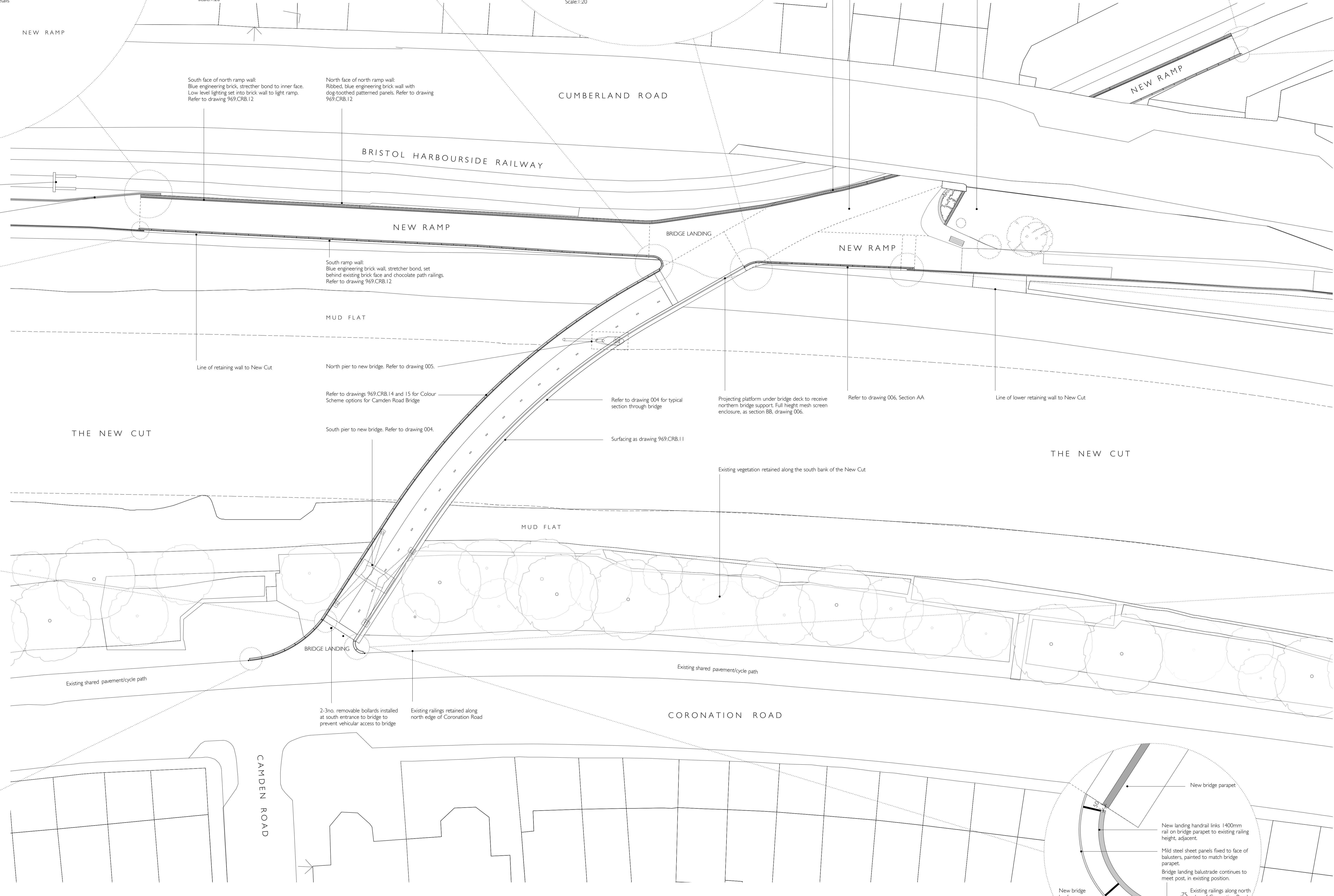
10.07 NORTH EAST END OF RAMP
End of ramp balustrading.
Scale: 1:20



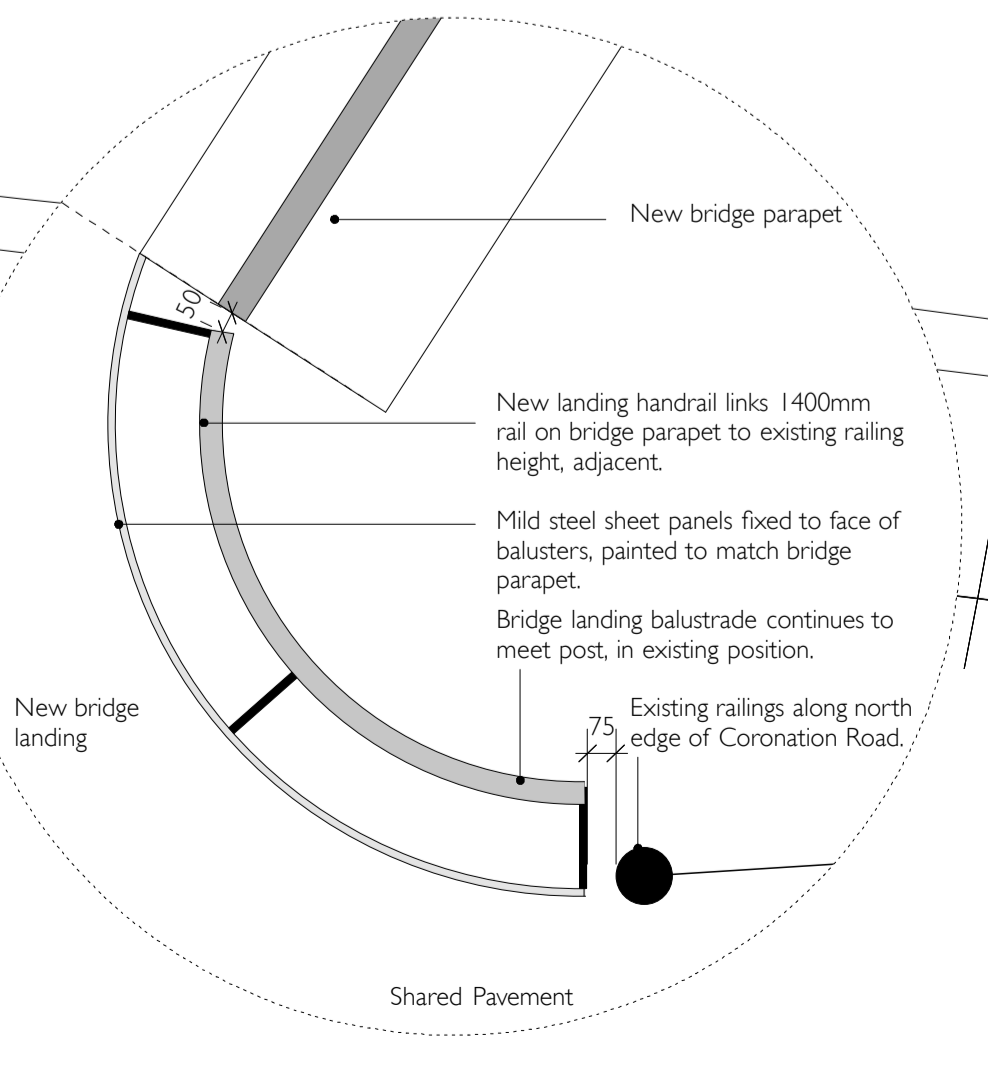
10.08 NORTH EAST RAMP
Junction between existing guarding on Chocolate Path and new balustrade.
Scale: 1:20



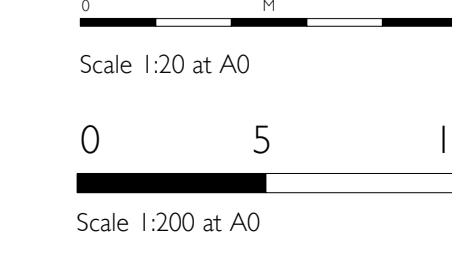
10.09 SOUTH EAST ENTRANCE
New balustrade, infilled to respond to parapet box handrail curves to meet height of existing railings.
Sketch not to scale

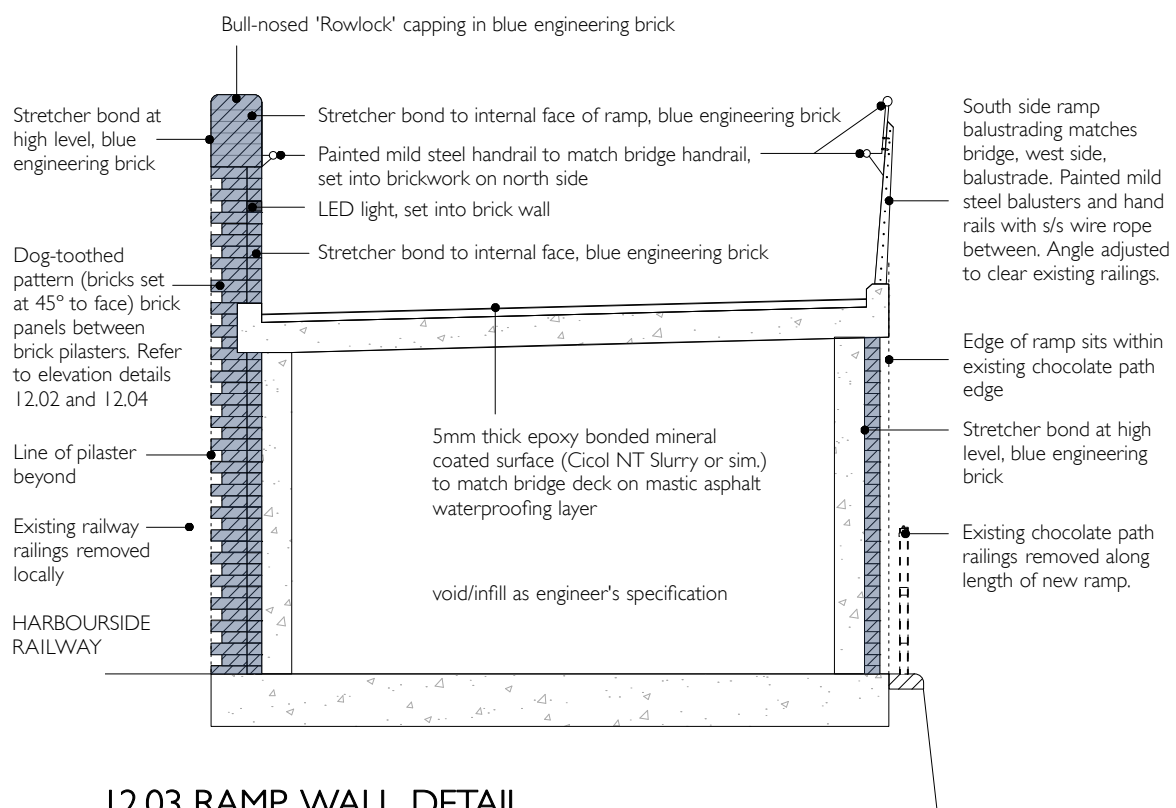


10.00 REFERENCE PLAN
Scale: 1:200 at A0

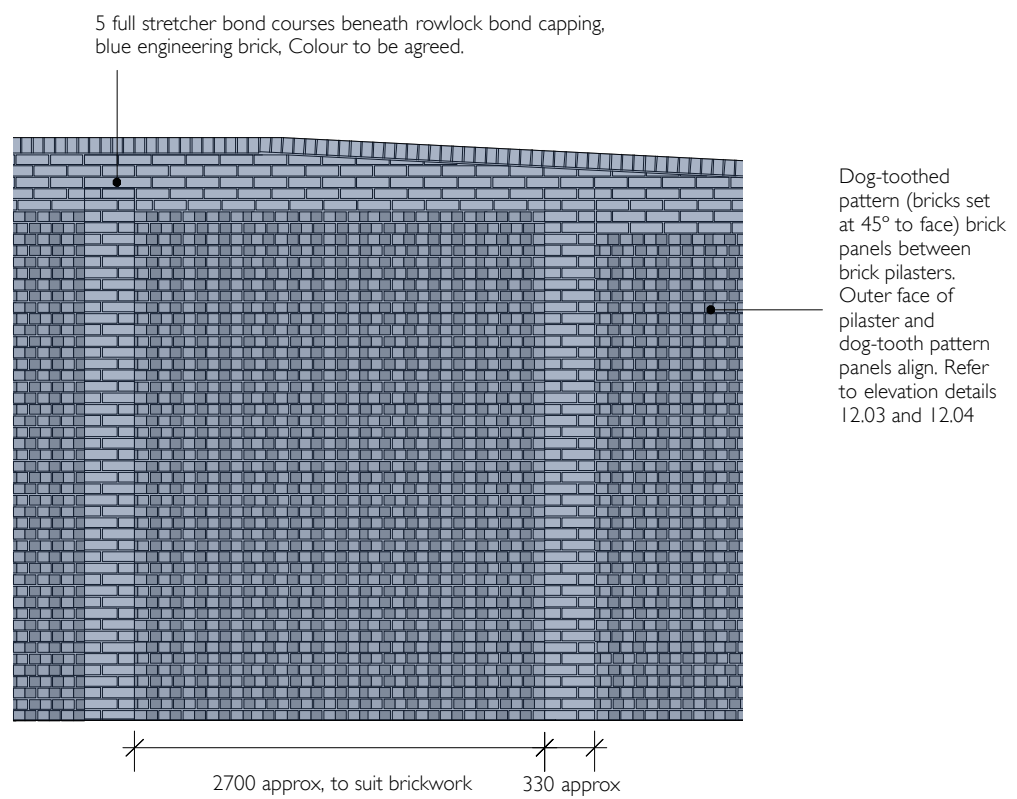


10.10 SOUTH EAST ENTRANCE
Junction between existing guarding, new balustrade and bridge structure.
Scale: 1:20





12.03 RAMP WALL DETAIL
Scale 1:50 at A3



12.04 NORTH WALL NORTH ELEVATION RAMP DETAIL
Scale 1:50 at A3



Scale 1:200 at A3



Scale 1:50 at A3

Rev A - 18th November 2014

Reference to existing chocolate path railings removed from ramp wall detail

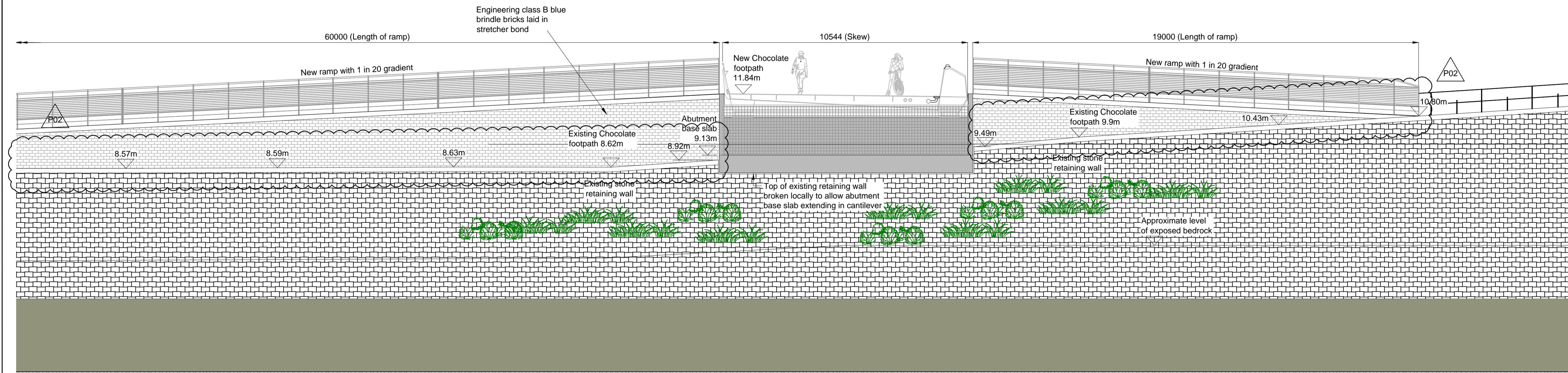
Cycling Ambition
for Bristol City Council

**CAMDEN ROAD BRIDGE
RAMP DETAILS**

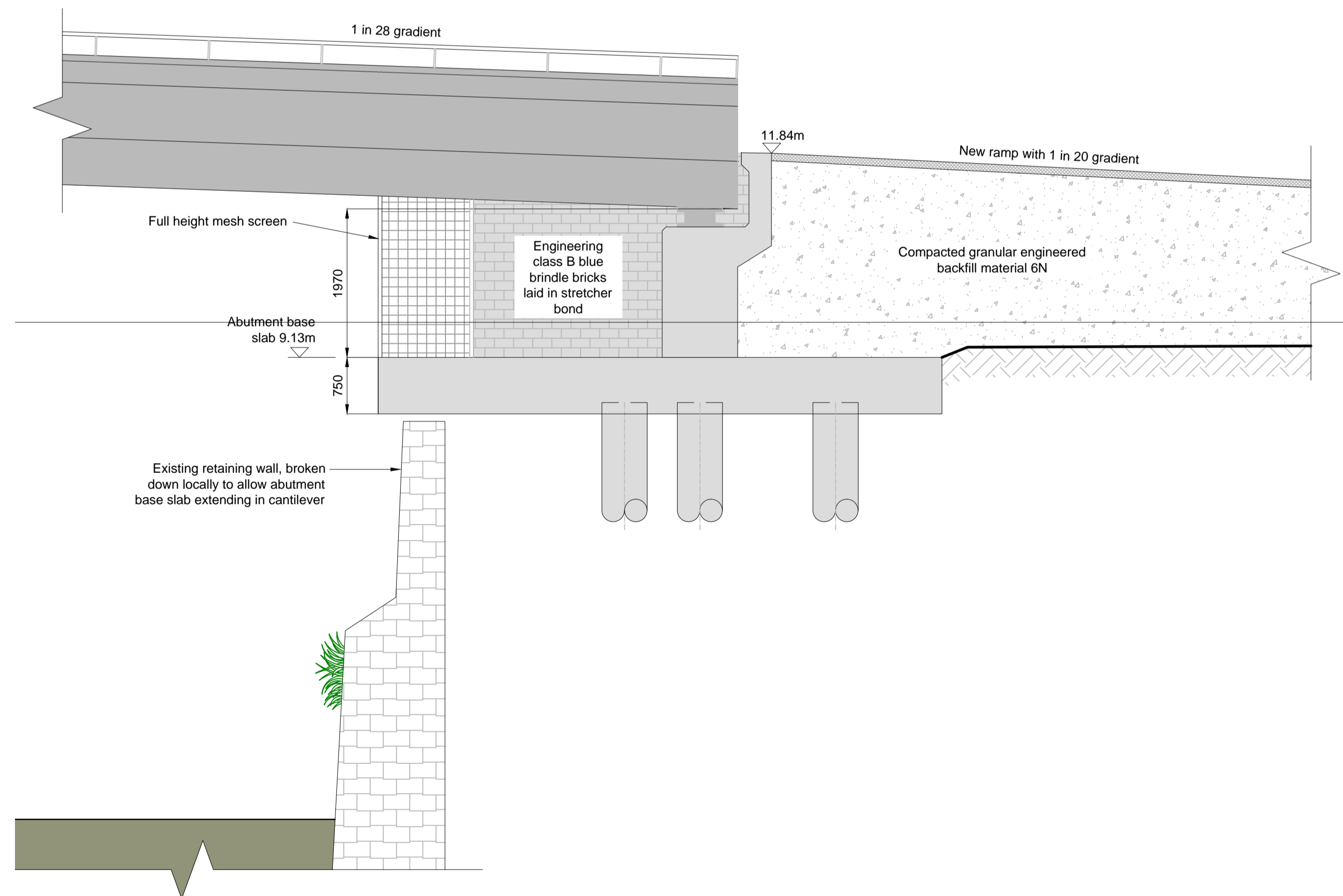
1:100 and 1:50 @ A3
August 2014

969.CRB.12A

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ELEVATION A-A
SCALE 1:100

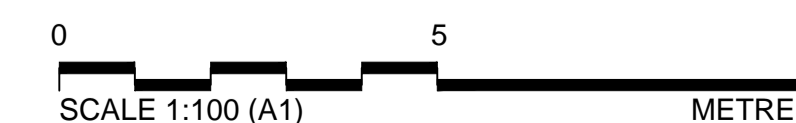


SECTION B-B
SCALE 1:50

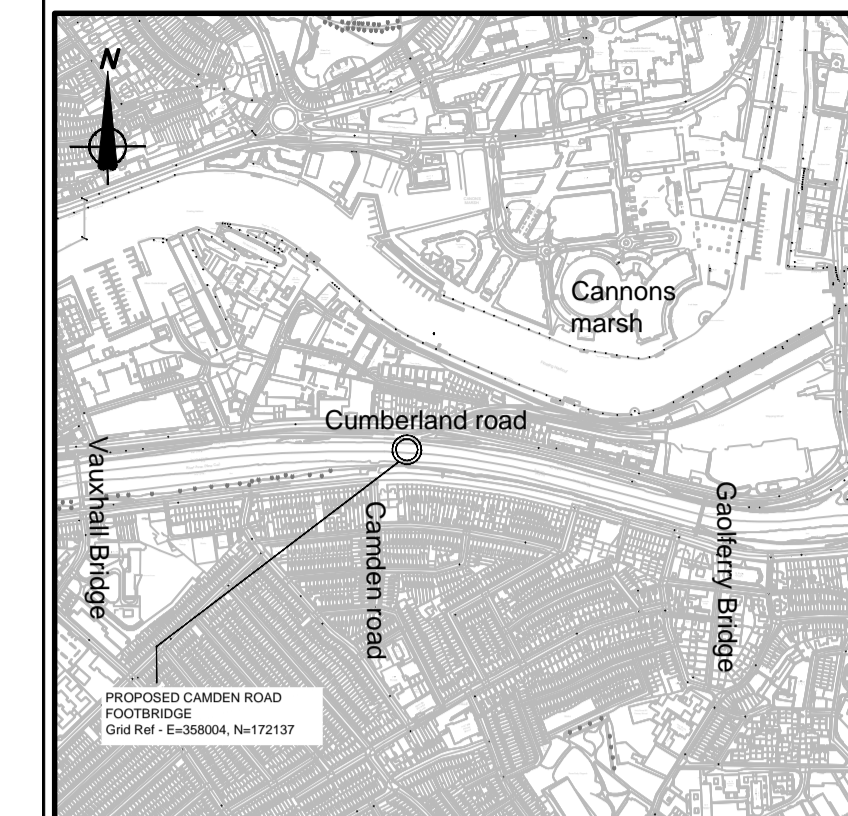


VIEW OF LANDING POINT
ON THE EMBANKMENT
NOT TO SCALE

FOR PLANNING



Location Plan:



NOTES

1. ALL DIMENSIONS ARE IN MILLIMETRES UNLESS STATED OTHERWISE.
2. ALL LEVELS ARE IN METRES AOD UNLESS STATED OTHERWISE.
3. FOR LOCATIONS OF SECTIONS A-A AND B-B REFER TO DRAWING NUMBER CRB-003.
4. DIMENSIONS AND DETAILS ARE INDICATIVE AND MAY BE SUBJECT TO MINOR CHANGES DURING DEVELOPMENT OF DETAIL DESIGN.
5. THIS DRAWING TO BE READ IN CONJUNCTION WITH DRAWING NUMBERS:

203742.BV.00.46.001	LOCATION PLAN
203742.BV.00.46.002	SITE CLEARANCE
203742.BV.00.46.003	PLAN AND LOCATION PROFILE
203742.BV.00.46.004	V-SHAPE PIER AND TYPICAL
DECK CROSS SECTION	
203742.BV.00.46.005	NORTH PIER
203742.BV.00.46.007	SECTIONS C-C, D-D AND
E-E	
203742.BV.00.46.008	NORTH RAMP AND BRIDGE
LEVELS	
969.CRB.10	REFERENCE PLAN & HANDRAIL
DETAILS	
969.CRB.11	SURFACE FINISHES
969.CRB.12	RAMP FINISHES & DETAILS
969.CRB.13	PLACEMAKING DETAILS
969.CRB.14	COLOUR OPTION 1-DARK
GREY	
969.CRB.15	COLOUR OPTION
2-GREY/BLUE	OPTION

02	JEC	DC	NF	11/11/14	Existing handrail removed
01	RM	BB	SP	22/08/14	Prepared for planning application
Rev	By	Chkd	Apprvd	Date	Description

Client



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Project
**BRISTOL CITY COUNCIL
CYCLING AMBITION FUND**

Drawing

**CAMDEN ROAD BRIDGE
SECTIONS A-A AND B-B**

Drawn by: RM Date: 22/08/14

Checked by: BB Date: 22/08/14

Approved by: SP Date: 22/08/14

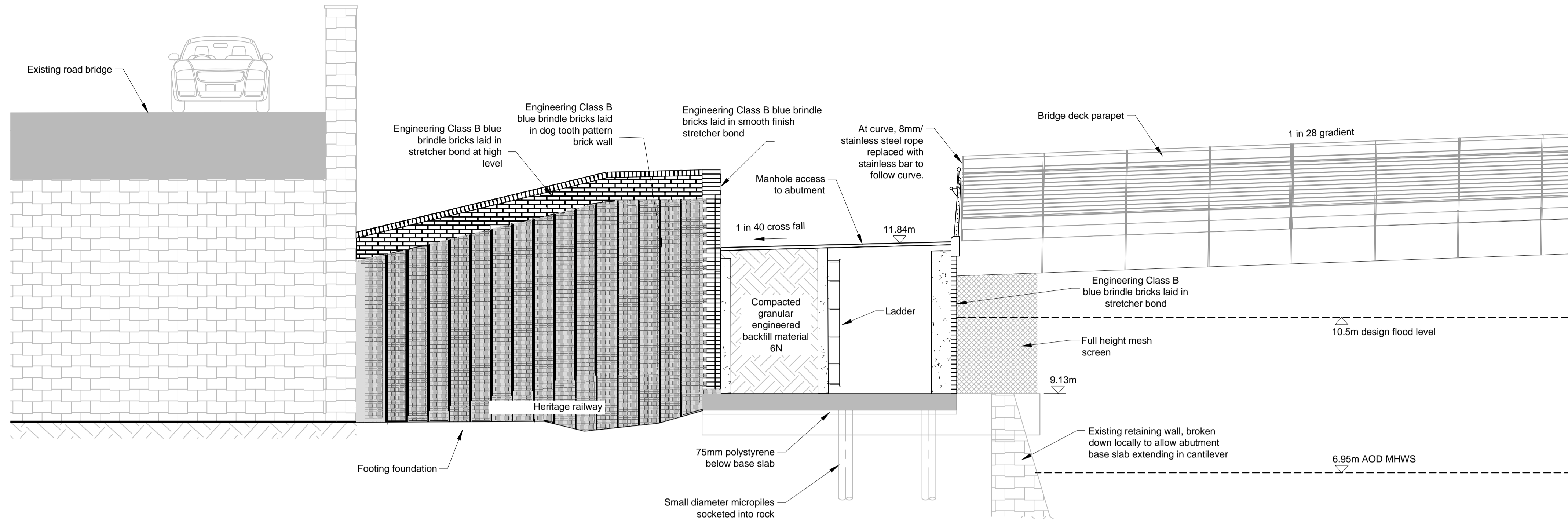
Drawing No. Revision

203742.BV.00.46.006

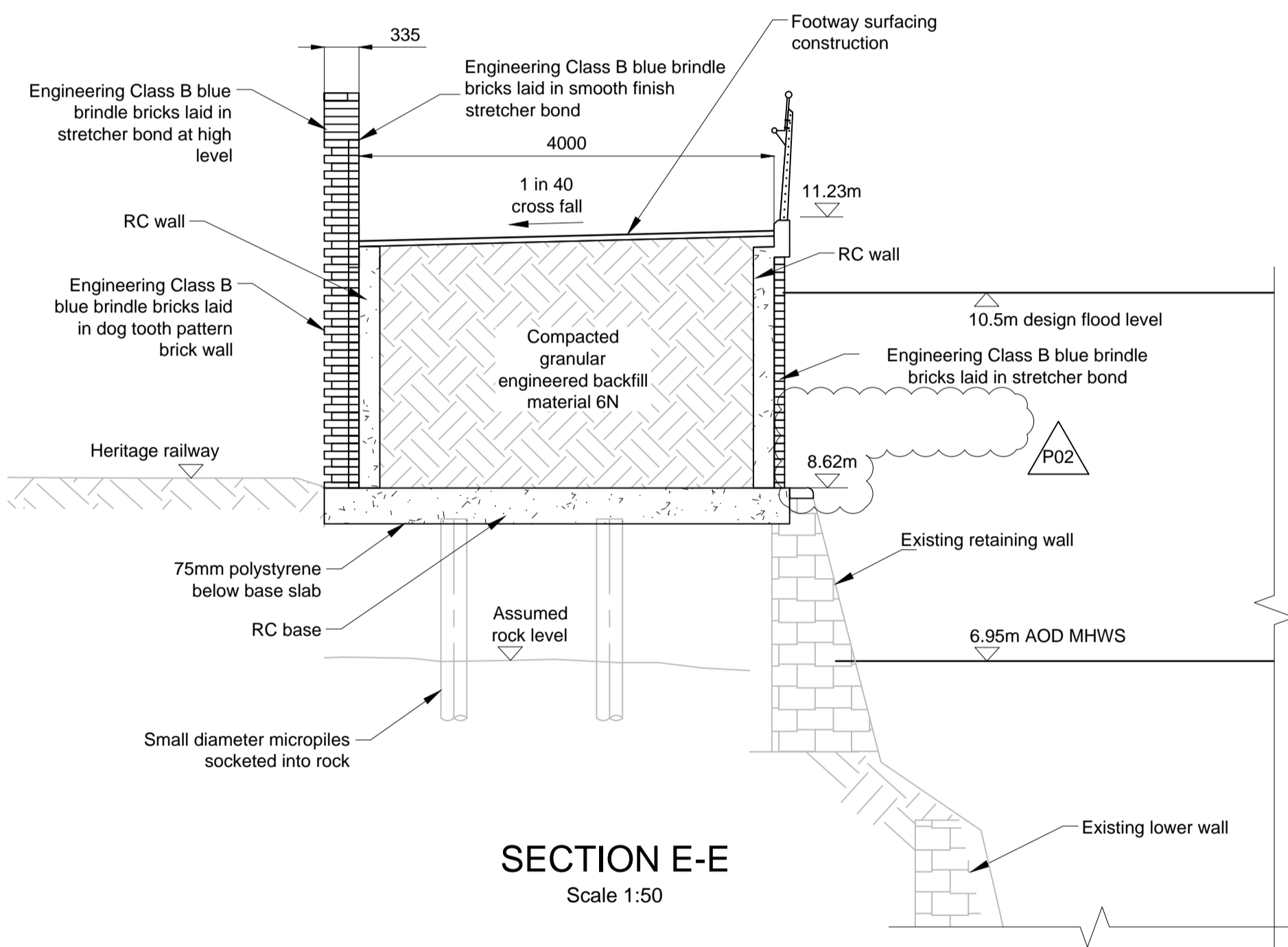
P 02

Drawing Scale: As shown at A1

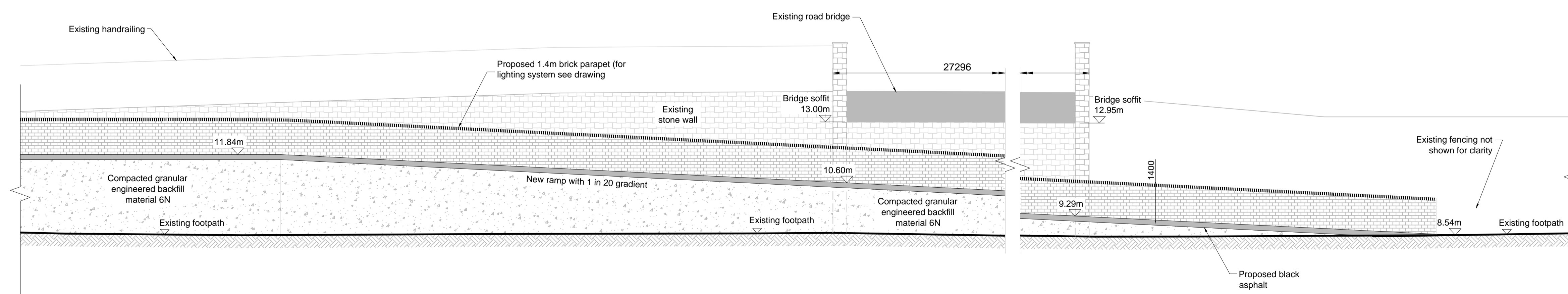
Drawing file path & name: C:\Users\jacobr1\Desktop\Cycling\203742.BV.00.46.006 - Section A-A and B-B.dwg
User and Plot Date: jacobr1: 11/11/2014 - 11:11 am



SECTION C-C
Scale 1:50

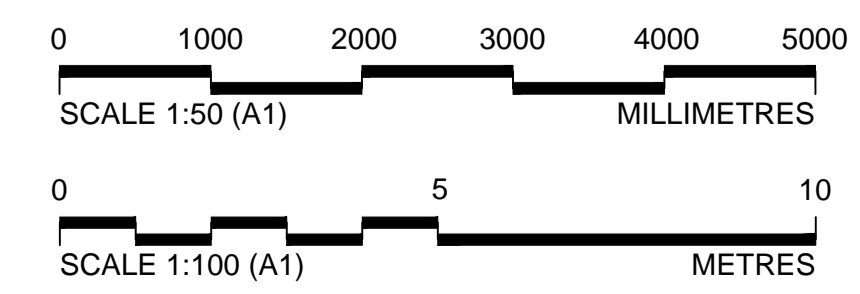


SECTION E-E
Scale 1:50

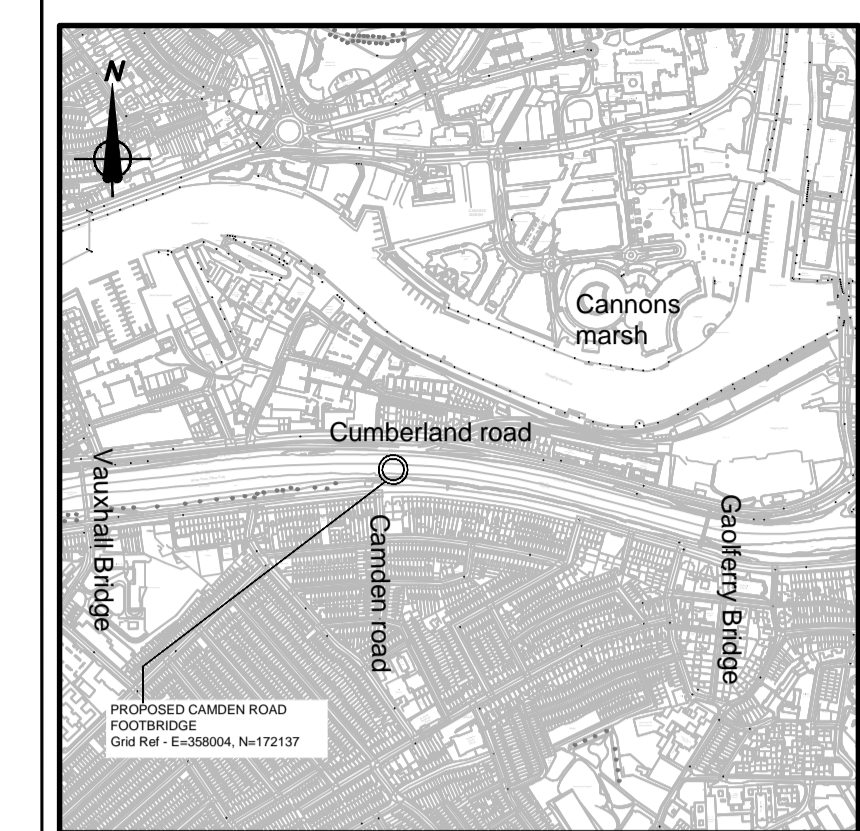


SECTION D-D
Scale 1:100

FOR PLANNING



Location Plan:



NOTES

1. ALL DIMENSIONS ARE IN MILLIMETRES UNLESS STATED OTHERWISE.
2. ALL LEVELS ARE IN METRES AOD UNLESS STATED OTHERWISE.
3. FOR LOCATIONS OF SECTIONS C-C, D-D AND E-E REFER TO DRAWING NUMBERS CRB-003.
4. DIMENSIONS AND DETAILS ARE INDICATIVE AND MAY BE SUBJECT TO MINOR CHANGES DURING DEVELOPMENT OF DETAIL DESIGN.
5. THIS DRAWING TO BE READ IN CONJUNCTION WITH DRAWING NUMBERS:

203742.BV.00.46.001	LOCATION PLAN
203742.BV.00.46.002	SITE CLEARANCE
203742.BV.00.46.003	PLAN AND LOCATION PROFILE
203742.BV.00.46.004	V-SHAPE PIER AND TYPICAL
203742.BV.00.46.005	DECK CROSS SECTION
203742.BV.00.46.006	NORTH PIER
203742.BV.00.46.007	SECTIONS A-A AND B-B
203742.BV.00.46.008	SECTIONS C-C, D-D AND E-E
203742.BV.00.46.009	NORTH RAMP AND BRIDGE
203742.BV.00.46.010	LEVELS
969.CRB.10	REFERENCE PLAN &
969.CRB.11	HANDRAIL DETAILS
969.CRB.12	SURFACE FINISHES
969.CRB.13	RAMP FINISHES & DETAILS
969.CRB.14	PLACEMAKING DETAILS
969.CRB.15	COLOUR OPTION 1-DARK
2-GREY/BLUE	COLOUR OPTION

Rev	By	Chkd	Apprvd	Date	Description
02	JEC	DC	NF	11/11/14	Existing handrail removed
01	RM	BB	SP	22/08/14	Prepared for planning application



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TEL: +44 (0)1793 812479 FAX: +44 (0)1793 812089

Project **BRISTOL CITY COUNCIL CYCLING AMBITION FUND**

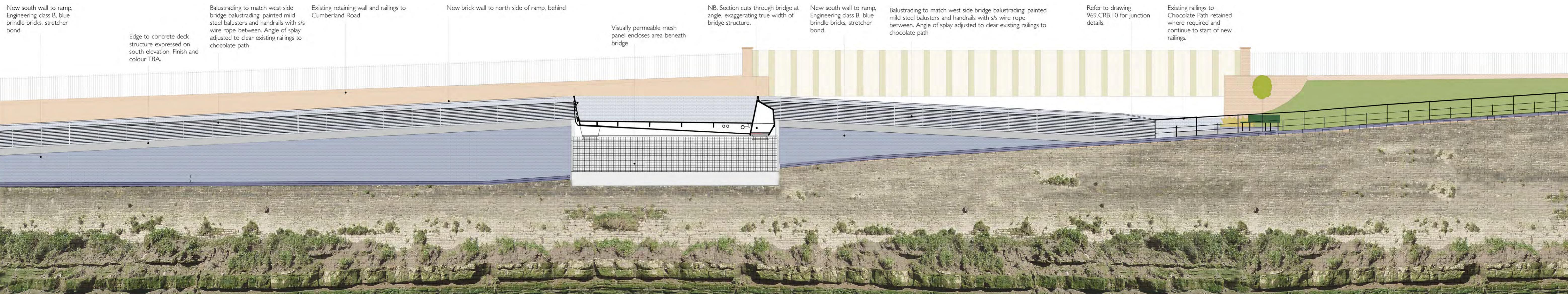
Drawing **CAMDEN ROAD BRIDGE SECTIONS C-C, D-D AND E-E**

Drawn by: RM	Date: 22/08/14
Checked by: BB	Date: 22/08/14
Approved by: SP	Date: 22/08/14

Drawing No.	Revision
203742.BV.00.46.007	P 02

Drawing Scale: As Shown at A1

Drawing file path: C:\Users\craut\Desktop\Cycling\203742.BV.00.46.007 - Section C-C, D-D and E-E.dwg
 User and Plot Date: jrabitt 11/11/2014 11:11 am



New south wall to ramp. Engineering class B, blue brindle bricks, stretcher bond.

Edge to concrete deck structure expressed on south elevation. Finish and colour TBA.

Balustrading to match west side bridge balustrading: painted mild steel balusters and handrails with s/s wire rope between. Angle of splay adjusted to clear existing railings to chocolate path

Existing retaining wall and railings to Cumberland Road

New brick wall to north side of ramp, behind

Visually permeable mesh panel encloses area beneath bridge

NB. Section cuts through bridge at angle, exaggerating true width of bridge structure.

New south wall to ramp. Engineering class B, blue brindle bricks, stretcher bond.

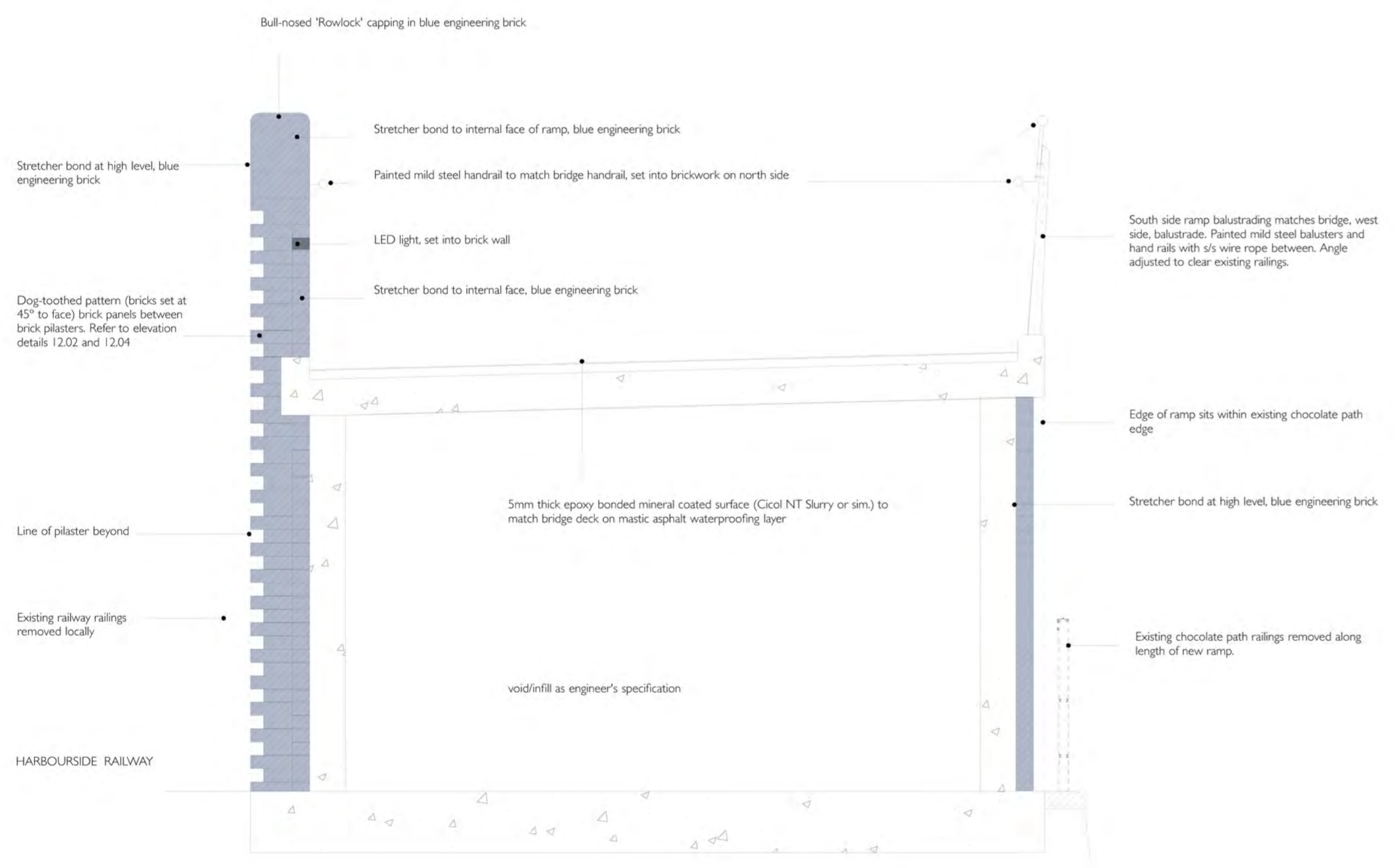
Balustrading to match west side bridge balustrading: painted mild steel balusters and handrails with s/s wire rope between. Angle of splay adjusted to clear existing railings to chocolate path

Refer to drawing 969.CRB.10 for junction details.

Existing railings to Chocolate Path retained where required and continue to start of new railings.

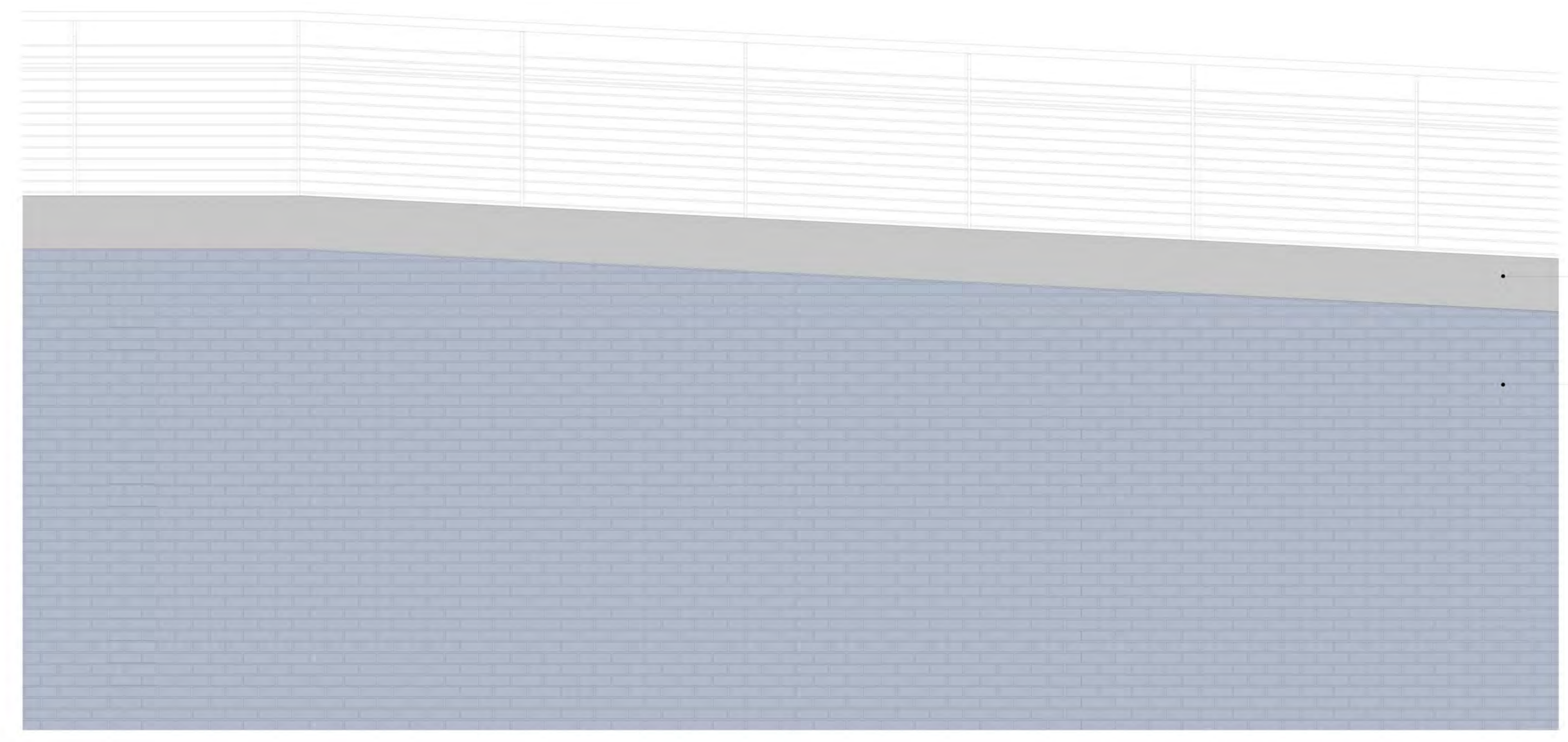
THE NEW CUT

16.01 RAMP WALL DETAIL, SOUTH ELEVATION
Scale 1:100 at A1



16.02 RAMP WALL DETAIL
Scale 1:25 at A1

Approx. line of bedrock.



16.03 SOUTH WALL SOUTH ELEVATION RAMP DETAIL
Scale 1:25 at A1

Rev B - 18th November 2014
Reference to existing chocolate path railings removed from ramp wall detail

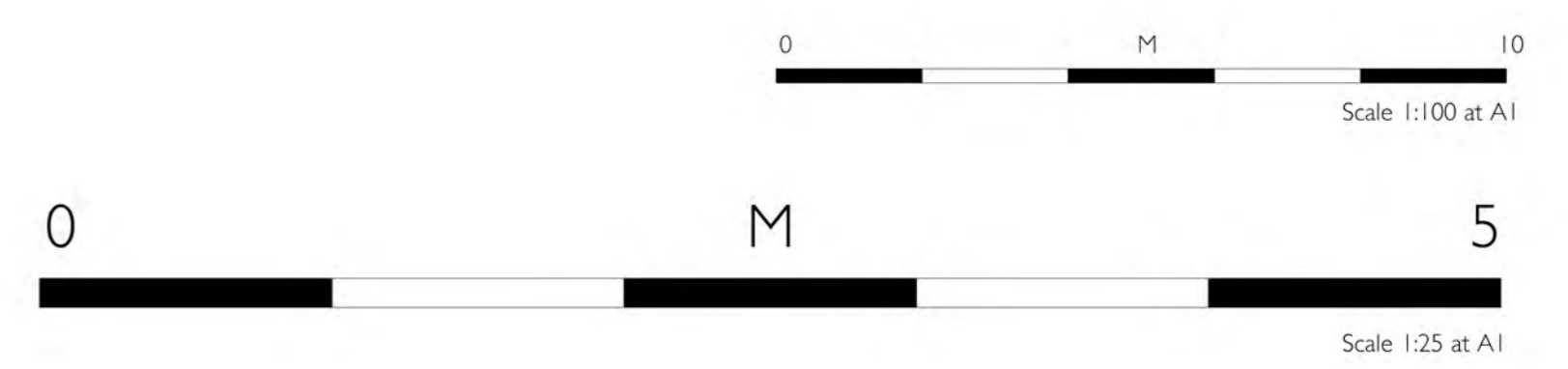
Rev A - 13th November 2014
Existing chocolate path railings below new ramp wall removed.

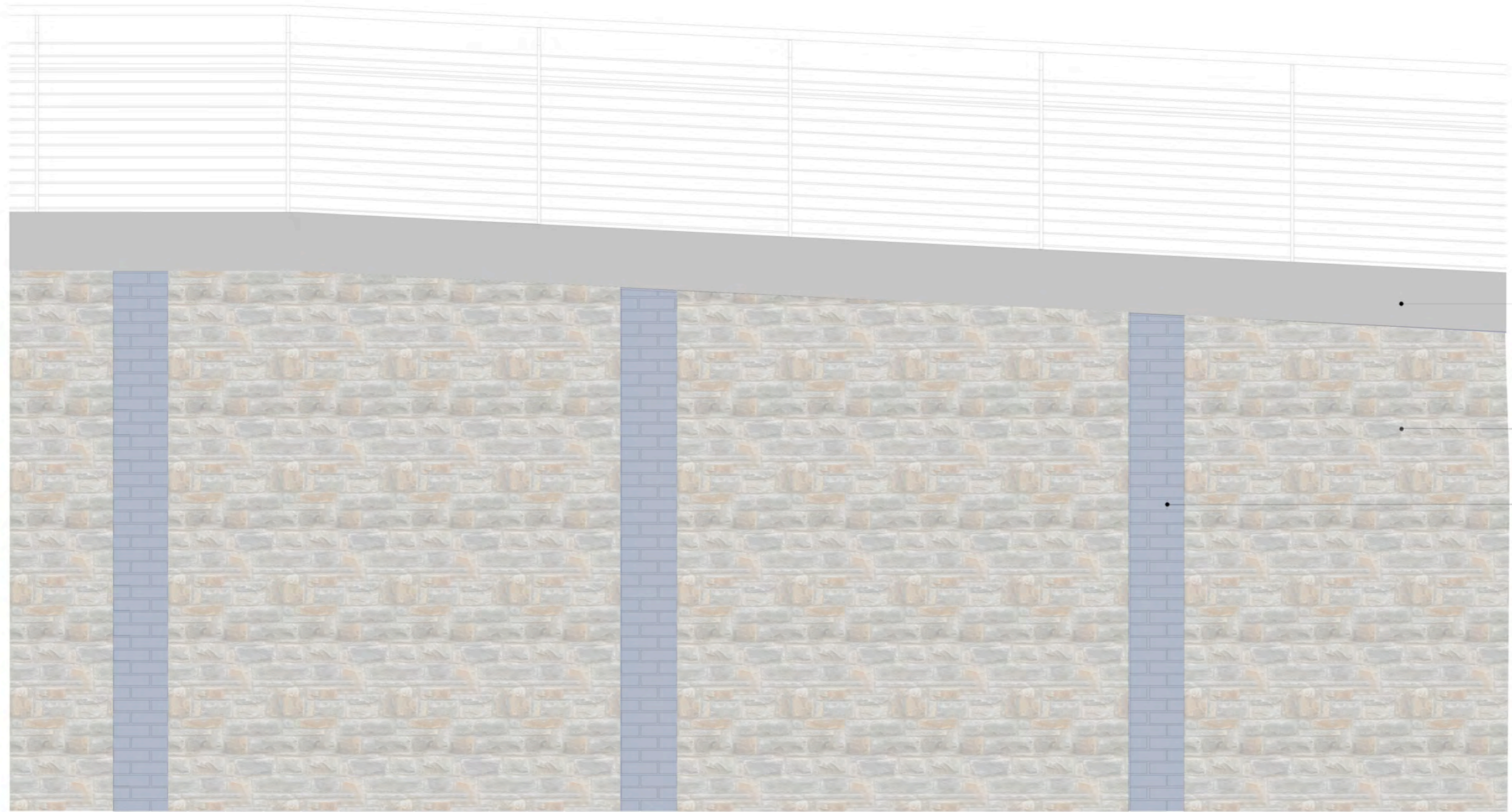
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SOUTH RAMP WALL ELEVATIONS & SECTION DETAIL
1:100 and 1:25 @ A1
October 2014

969.CRB.16B

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Concrete upstand to edge of walkway. Colour and finish to be agreed.

Pennant stone infill panels

Blue brindle brick pilasters

17.01 SOUTH WALL SOUTH ELEVATION
Scale 1:25 at A3



Scale 1:25 at A1

Cycling Ambition
for Bristol City Council

**SOUTH RAMP WALL
ELEVATION SHOWING
PILASTERS & PENNANT
STONE**
1:100 and 1:25 @ A1
October 2014

969.CRB.17

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